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# MT. HOOD NATIONAL SCENIC BYWAY INTERPRETIVE PLAN & DESIGN GUIDELINES



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## INTRODUCTION

The Mt. Hood National Scenic Byway was officially designated in September 2005. The route spans 105 miles, encompassing the south, west, and east sides of Mt. Hood from I-84 in Troutdale along highways 26, 35, and 30 to the Columbia River Gorge. The Byway traverses three counties (Multnomah, Clackmas, and Hood River) and includes small rural communities, pastoral farmland, and forested mountains.

Though only recently designated as a National Scenic Byway, the route is part of the “Mt. Hood Loop” and has been a favorite for over 100 years. The loop includes the I-84 section of the National Scenic Columbia River Gorge from Portland to Hood River, and portions of the Historic Columbia River Highway.

The highlight of the experience is the mountain itself. Towering over the area at 11,239 feet, it is the highest mountain in Oregon and the crown jewel of the northern Oregon Cascade Range. The west slope temperate rainforest receives over 100” in precipitation a year, creating world class recreational opportunities

including the longest season of alpine skiing in North America. On the east slope lower precipitation combines with the rich volcanic soil making Hood River Valley an agricultural giant for fruits and vineyards.

### Purpose and Need

The biggest challenge of this planning effort is to tie together various sites, venues, and opportunities along the Byway, and package them in a strategic and consistent way. Currently, there are several individual efforts that highlight various parts of the route, but no cohesive identity for the Byway. This plan will:

1. Identify storylines that tie the Byway together.
2. Provide design guidelines that will create visual theme and sense of place.
3. Make prioritized recommendations for implementation with Byway partners.

Inspiration for the stories and design guidelines will come from the natural and cultural history of the area and existing iconic images of Mt. Hood, regional agriculture, recreation, and the Cascadian architecture of Timberline Lodge. Particular emphasis will be given to the entrance portals near the communities of

Troutdale and Hood River in order to give visitors a sense of arrival. We want them to know they are on the Mt. Hood National Scenic Byway. As they continue along the route there will be additional iconic images and structures that will reassure them that they are still on the Byway.

**Parameters and Planning Assumptions**

This plan is based on a number of assumptions about the current and anticipated future use of the area. Significant changes in visitor use, partners or management policies may require modifications to the plan. The following assumptions serve as a foundation for this plan:

- This interpretive plan is consistent with the Mt. Hood Scenic Byway Corridor Management Plan.
- The Mt. Hood Scenic Byway Steering Committee primary partners consist of:
  - Clackamas County Tourism and Cultural Affairs
  - Hood River County Chamber of Commerce
  - West Columbia Gorge Chamber of Commerce
  - Mt. Hood National Forest
- The steering committee will continue to cooperate with federal and state land

management agencies, the Oregon Dept. of Transportation, businesses, Native American Tribes, special use permittees, and other partners.

- Interpretive services will focus on providing quality visitor experiences, meeting identified demand, mitigating impacts to cultural, historical and natural resources, and helping to diversify local economies.
- The partners working cooperatively along the Byway will work to find consistent and reliable funding sources to ensure that Byway interpretive facilities and services are quality products, well maintained and updated for the traveling public.
- Specific site plans may need to be developed for each portal, information center, and proposed wayside. Universal accessibility, winter access, and highway site distances are especially important.
- Portals will provide visitor information and orientation and sense of arrival.
- This plan will seek to enhance and support existing tourism efforts, such as the Fruit Loop and the Mt. Hood Loop tours.

## GOALS AND OBJECTIVES

The overall goal of this interpretive plan is to communicate, through interpretive media, those values that engender appreciation and understanding, and instill stewardship for the unique resources and history of the region and Mt. Hood Scenic Byway Corridor. Both management and visitor experience goals and objectives are shown below.

### Management Goals

- Establish consistent guidelines for Byway interpretation and design; coordinate various interpretive media formats to present consistent and complete interpretive stories of the area
- Promote unique and special aspects of individual communities to increase tourism (particularly overnight visits) and commerce
- Foster conservation of natural and cultural resources of the area
- Promote cooperation among the various groups (agencies, communities, business and organizations) involved with the Byway by:
  - Agreeing as partners on signing, facility design, and interpretive sites
  - Partnering on future grants

- Providing a broad range of facilities (restrooms, picnic areas, trailheads, pull-offs, water) to meet visitor needs
  - Connecting visitors to the land through information and interpretation on recreation opportunities, agritourism, and cultural/natural history of the area
  - Participating in partners' planning and project development activities to address Mt. Hood Scenic Byway objectives and to explore opportunities to develop joint projects.
- Share information with visitors on safety tips, land use ethics and regulations, and the rationale behind them

### Management Objectives

By using this plan, managers will be able to:

- Provide a theme and subthemes to link the varied sites of the byway
- Establish two primary entrance portals: 1) near Troutdale, and 2) near Hood River
- Establish two secondary entrance portals: 1) Mt. Hood/Parkdale, and 2) Zigzag Ranger District
- Identify interpretive media products and priorities for the byway experience

- Recognize partnerships at participating sites and facilities

**Visitor Experience Goals**

The following goals identify desired visitor experiences and outcomes, and should be considered in planning for specific sites, exhibits, and projects. These goals include:

- Provide orientation and information that help visitors explore the corridor and access the various opportunities
- Provide visitors with high quality facilities designed to meet their needs and expectations
- Provide opportunities for on-site interpretation and exploration while protecting important cultural, historical, wilderness, and natural resources values

**Visitor Experience Objectives**

As a result of implementing this plan, visitors will:

- Recognize the significant relationship that exists between the land and people, through emotional, social and/or intellectual experiences
- Develop a stewardship ethic for natural

and cultural resources by supporting various management policies, and not littering or vandalizing

- Support the local economic base
- Access information allowing them to participate in various opportunities such as special events and festivals
- Return - because of their positive experiences
- Appreciate the unique and significant aspects of various communities and natural resources the Byway traverses

## VISITOR USE AND MARKETING STRATEGIES

Based on Longwoods International Studies about visitors to the Mt. Hood/Columbia River Gorge Region, and the Forest Service Visitor Use Monitoring Study (NVUM, 2004), the following summary has been compiled about visitors to the Mt. Hood area:

- Annual visitation to the Mt. Hood/Gorge Region is 2.2 million or 10% of the Oregon tourism market
- 55% of the visitors spent at least one night in the area
- 45% of the visitors were day use only
- 50% were in the area principally for pleasure
- 41% were visiting friends and relatives
- 9% were on business
- Roughly 75% of the visitors live within a 150 mile radius
- 1.4% of the visitors were international
- Recreation was the primary reason for visiting the area. Most common activities included:
  - Relaxing
  - Viewing wildlife
  - Hiking/walking
  - Viewing natural features

- Driving for pleasure
- Fishing
- Downhill skiing
- Visiting Timberline Lodge

To date there has not been a study commissioned specifically for the Byway. However, business owners along the Byway have observed that:

- A major segment of their visitors are convention participants (from Portland)
- During the summer they have more national and international visitors
- During winter, local and regional visitors tend to predominate

### Marketing Strategies

Extensive marketing efforts are on-going with Byway partners. These strategies have been outlined in the Mt. Hood Scenic Byway Corridor Management Plan (Appendix B). This interpretive planning effort will tier to these strategies.

It is recommended that Byway proponents draw upon the design elements contained in this plan (fonts, color palettes, panel shapes and layouts) for use in future marketing materials.

## NOTES

## INTERPRETIVE OVERVIEW

### Statements of Significance

Statements of significance capture the unique nature and intrinsic qualities of Mt. Hood Scenic Byway that are important to the Byway partners and the visitors. What's so special about the area? What makes it distinctive on a state, regional, or national scale? Why does it inspire people? The answers to these questions help set the stage for the development of themes and storylines.

The Mt. Hood Scenic Byway Corridor Management Plan Description of Intrinsic Qualities (Appendix A) and the Mt. Hood National Forest Recreation Niche (available on the Forest's website: [www.fs.fed.us/r6/mthood](http://www.fs.fed.us/r6/mthood)) highlight the significant resources found throughout the area. Based on this foundation the following theme, subthemes and storylines have been developed.

### Mt. Hood National Scenic Byway Theme and Storylines



Majestic Mt. Hood dominates the landscape and its character shapes the land, people, and history of the area.

**Subtheme 1: The active geological processes, topography, and hydrology of Mt. Hood continue to influence the landscape and ecological systems of the area.**

- A. Storyline:** Mt. Hood is a slumbering volcano, with venting fumaroles, deformation and debris flows (caused by melting glaciers and heavy rainfall). Scientists expect the mountain will have another eruption sometime in the future, although smaller than what has previously occurred.
- B. Storyline:** The massive scale of the mountain itself influences vastly different climatic conditions on its east and west flanks. Orographic uplift on the mountain's western side induces rainfall exceeding 100 inches/year. The Byway bisects the resultant temperate rainforest. The mountain creates a rain shadow on its eastern side, receiving about 40 inches/year in the Hood River Valley.
- C. Storyline:** Mt. Hood is the headwaters for many magnificent rivers including Hood River and the Wild and Scenic Rivers of upper Sandy, White, and Salmon.
- D. Storyline:** In 1892 the Bull Run Forest Reserve was established for the purpose of providing a continuous supply of timber and water. In 1895 the Bull Run became Portland's water supply. Over the years there have been many Congressional Acts regarding the restriction and access of the area.
- E. Storyline:** Mt. Hood's eastern slope experiences sunnier days, drier climate and a host of micro-climates. The climate, combined with the rich volcanic soil and irrigation water from Mt. Hood, has made Hood River Valley one of the most fertile and productive agricultural communities in the United States.
- F. Storyline:** Salmon and trout fisheries in the Columbia River Basin, including the rivers that surround Mt. Hood, have been and continue to be important ecological, cultural, and economic resources. Balancing the habitat needs of fish and the many water needs of people (for cities and for agriculture) is a significant and dynamic challenge.

**Subtheme 2: Mt. Hood's looming presence presents both challenges and abundant resources for an expanding American nation.**

- A. Storyline:** Native American tribes utilized the area's resources for centuries, making annual visits to the high meadows and lakes to gather berries, hunt wild game, fish, and for social interactions with other tribes.
- B. Storyline:** The essence of Mt. Hood's character is captured in the Native American legend of Wy'east.
- C. Storyline:** Lewis and Clark camped and explored the area during 1805 and 1806. Most notable are their journal notes on the Sandy or Quicksand River (the names reflecting the nature of the river, due to a volcanic eruption in 1790). Their journal notes have provided modern scientists with a glimpse into the past and have helped them with their ecological studies of Mt. Hood.
- D. Storyline:** For emigrant travelers on the Oregon Trail the Cascade Mountains were the final obstacle before reaching what they referred to as the "promised land." The opening of the Barlow Road in 1846 completed the Oregon Trail as a land route, and became the first toll road over the Cascades. It was also considered the most treacherous because of the infamous Laurel Hill chute.
- E. Storyline:** In the fall of 1849 members of Lieutenant William Frost wagon train were forced to abandon about 45 wagons on Camp Creek due to deteriorating conditions of the livestock, men, and weather. The area became known as the Government Camp on Camp Creek and eventually shortened to Government Camp.
- F. Storyline:** Over the years the Barlow Road was improved and by the 1880 residents in nearby communities were using it to recreate on Mt. Hood. In 1915 the State of Oregon acquired the right-of-way, removed the toll road and began constructing what would be known as the Mt. Hood Loop Highway. Today, portions of the Barlow Road and the historic Mt. Hood Loop Highway are part of US Highway 26, while other portions remain intact and accessible.

**G. Storyline:** Transportation on the mountain has evolved throughout the years reflecting the people and the times. Native American followed animal paths, which the emigrants later followed and improved upon creating the Barlow Road (wagons), parts of which later became the Mt. Hood Loop Highway for early automobiles. Eventually the Mt. Hood railroad and inter-urban trolley were constructed to transport both people and cargo. Today there is a modern highway (modern vehicles) allowing visitors greater access than ever before. However, even in this time there is a growing trend to slow down and reconnect with the land by creating the Springwater Corridor, an inter-urban bike path between Portland to Mt. Hood and the surrounding communities, utilizing the old inter-urban trolley rail bed

**H. Storyline:** The Oregon National Forest was established in 1908 when the Bull Run Reserve (1892) and portions of the Cascade Range Forest Reserve (1893) were combined. Later in 1924, the forest was renamed the Mt. Hood National Forest.

**I. Storyline:** The location of Troutdale was first documented by Lt. Broughton, a British naval officer, in 1792. It wasn't until 1872 that the town received its name from Captain John Harlow, a former sea captain from Maine, who raised trout on his farm named Troutdale.

**J. Storyline:** During the Great Depression of the 1930s, Franklin D. Roosevelt established the New Deal, part of which included putting unemployed workers to work on public works projects and providing them with a life-sustaining wage. Mt. Hood National Forest hosted the Zigzag Civilian Conservation Corps (CCC) Camp and Summit Meadow Works Progress Administration (WPA) Camp. These men and women provided the labor for many projects on the national forest. Timberline Lodge was and remains the most well-known. In 1972 it was added to the National Register of Historic Places, in recognition of its architectural and historic significance, and the people who created it.

**K. Storyline:** During the Great Depression, "Poor Farms" were established to help families survive. Edgefield, located in Troutdale, was one such place due to its mild climate. Later it became a nursing home and today it is a destination resort and national historic landmark operated by McMenamins Hotels and Pubs.

**L. Storyline:** Logging is an important ecological and economic enterprise in the area. The decline of logging on national forest lands in the 1990s reduced revenue for schools and public programs such as law enforcement, fire and libraries, creating hardships on rural communities. In 2002, some private forest lands in Hood River County became a certified Tree Farm with the American Tree Farm System. This certification recognizes sustainable and exemplary forestry. In addition, Hood River County has several thousand acres suitable for timber production. Revenues generated from the timber sales helps off-set lost property taxes.

**M. Storyline:** Clackamas County proclaims itself the “Christmas tree capital of the world.” Numerous plant nurseries, which ship ornamental and garden plants throughout the United States, are also a vital part of the county’s economy.

**N. Storyline:** The salmon have been an integral part of the pacific Northwest Native American way of life; they continue to play a significant role in their spiritual and cultural heritage.



**Subtheme 3: Mt. Hood continues its long-standing tradition as a place for people to recreate.**

**A. Storyline:** Climbing and hiking Mt. Hood are popular recreational activities. As early as the 1840s, climbers were attracted to Mt. Hood and in 1854 the first documented ascent was accomplished on the south side by Thomas J. Dryer, publisher of the Weekly Oregonian. Timberline Trail, a 40-mile trail encircling Mt. Hood, was constructed in 1933-35 and features six stone shelters. (The trail is currently washed out.)

**B. Storyline:** In 1916 the Forest Service built the Timberline Cabin to serve as an emergency summer and winter shelter. Later, in 1924 they built a larger tent-hotel (Hotel at Timberline). Finally, in 1937, Timberline Lodge was constructed and remains in operation today serving skiers, climbers, hikers, and sightseers.

**C. Storyline:** Heavy snowfall and late season melting makes Mt. Hood the ideal location for winter sports. Mt. Hood has five ski area, several snow parks, and the longest ski season in North America.

**D. Storyline:** Many residents of Portland and other nearby communities make frequent visits to the mountain to camp, picnic, hike, mountain bike, fish, kayak, have family gatherings, pick huckleberries and wild mushrooms, gather firewood and rocks, and cut their Christmas tree. Mt. Hood National Forest offers outstanding scenic beauty, waterfalls, rivers, lakes, hiking trails (including a portion of the Pacific Crest National Scenic Trail) and significant cultural sites including the Barlow Road/Oregon Trail.

**E. Storyline:** The Byway encompasses portions of the Oregon Cascades Birding Trail.

**F. Storyline:** The region provides a cornucopia of agritourism opportunities, including orchards, wineries, u-pick farms, plant nurseries, and culinary delights. Visitors can enjoy the Hood River Valley on the Historic Mt. Hood Scenic Railway.

**G. Storyline:** The art community is vibrant and growing, inspired by the natural beauty and history of the area, cultural centers, museums, art studios, and galleries abound.

**H. Storyline:** Because of its ruggedness, weather extremes, and high elevation, the mountain can be unforgiving. The area remains wild and untamed. Visitors to the backcountry need to be knowledgeable and skilled in wilderness survival techniques.

## EXISTING AND PROPOSED INTERPRETIVE MEDIA

### Primary Entrance Portals and Visitor Information Centers

The entrance portals were selected for their ease in locating and in an attempt to separate the two byways (Mt. Hood and the Historic Columbia River Highway) in order for each of them to have their individual identities. Visitors traveling the Byway will be able to clearly identify—through design features—that they have arrived.

Site	Location	Purpose/ Objective	Existing Media/ Facilities	Recommended Media/ Facilities	Priority
West Entrance Portal	Route changes are being considered here. Donald L. Robertson Park, McMenamins Edgefield, or another site TBD may be the best option, depending on the new alignment.	<ul style="list-style-type: none"> <li>• Provide a sense of arrival</li> <li>• Orient visitors to the Byway and its many opportunities</li> <li>• Introduce visitors to one of the Byway’s major attractions</li> </ul>	None	<ul style="list-style-type: none"> <li>• Approach pillars/marker</li> <li>• Entrance kiosk with restrooms and drinking fountain</li> <li>• 3- 36”x 48”x 1/8” high pressure laminate panels                             <ul style="list-style-type: none"> <li>• Orientation map of Byway</li> <li>• Welcome panel</li> <li>• Commercial opportunities (dining, lodging, shopping, agriculture, outfitters, skiing, etc.) Encouraging visitors slow down and stay awhile.</li> </ul> </li> </ul>	High
East Entrance Portal	TBD. Ideally, site should be a bit beyond Hood River, capture the views, and include vistas of rural areas. Reconsider Panorama Point.	<ul style="list-style-type: none"> <li>• Provide a sense of arrival</li> <li>• Orient visitors to the Byway and its many opportunities</li> </ul>	None	<ul style="list-style-type: none"> <li>• Approach pillars/marker</li> <li>• Entrance kiosk with restrooms and drinking fountain</li> <li>• 3- 36”x 48”x 1/8” high pressure laminate panels                             <ul style="list-style-type: none"> <li>• Orientation map of Byway</li> <li>• Welcome panel</li> <li>• Commercial opportunities (dining, lodging, shopping, agriculture-Fruit Loop)</li> </ul> </li> </ul>	High

EXISTING & PROPOSED  
INTERPRETIVE MEDIA

**Secondary Entrance Portals**

Site	Location	Purpose/Objective	Existing Media/Facilities	Recommended Media/Facilities	Priority
Zig Zag Ranger Station	Zig Zag	<ul style="list-style-type: none"> <li>Provide information and orientation to the Mt. Hood NF, particularly the Zig Zag Ranger District and Mt. Hood National Scenic Byway</li> <li>Provide collection permits and recreation passes</li> </ul>	<ul style="list-style-type: none"> <li>Restrooms</li> <li>Staffed reception area</li> <li>Interior sales outlet and tourist info</li> <li>Exterior kiosk</li> </ul>	<ul style="list-style-type: none"> <li>This site will replace the Mt. Hood Visitor Information Center</li> <li>The facility will undergo a major remodeling of its lobby and exterior kiosk</li> <li>New interior exhibits will focus on recreation opportunities and the natural and cultural history of the area</li> <li>Exterior kiosk panels will include:                             <ul style="list-style-type: none"> <li>Orientation map of Byway</li> <li>Welcome panel</li> <li>Commercial opportunities</li> </ul> </li> </ul>	High
Hood River Ranger Station	Mt. Hood/Parkdale	<ul style="list-style-type: none"> <li>Provide information and orientation to the Mt Hood NF, particularly the Hood River Ranger District and Mt. Hood National Scenic Byway</li> <li>Provide collection permits and recreation passes</li> </ul>	<ul style="list-style-type: none"> <li>Restrooms</li> <li>Staffed reception area</li> <li>Interior sales outlet local recreation info</li> <li>Exterior mountain geographic feature locator</li> </ul>	<ul style="list-style-type: none"> <li>The facility will undergo a major remodeling of its lobby and exterior kiosk.</li> <li>New interior exhibits will focus on recreation opportunities, natural and cultural history with particular emphasis on the ditches/watershed mangement and its role with the agriculture of Hood River</li> <li>Exterior kiosk panels will include:                             <ul style="list-style-type: none"> <li>Orientation map of Byway</li> <li>Welcome panel</li> <li>No commercial panel</li> </ul> </li> </ul>	High

**Tertiary Information Centers**

Site	Location	Purpose/Objective	Existing Media/Facilities	Recommended Media/Facilities	Priority
Mt Hood Cultural Center and Museum	Government Camp	<ul style="list-style-type: none"> <li>Preserve and showcase the history of Government Camp and promote the recreational legacy</li> <li>Provide a venue for arts and educational lectures</li> </ul>	<ul style="list-style-type: none"> <li>Six galleries that feature exhibits of local history and art displays</li> <li>Restroom and drinking fountain</li> <li>Mt. Hood Territory TeleTales installment (<a href="http://www.mthoodterritory.com/teletales.jsp">www.mthoodterritory.com/teletales.jsp</a>)</li> </ul>	<ul style="list-style-type: none"> <li>Provide exterior signage for the building - work in conjunction with current planning efforts</li> <li>Per ongoing community discussions, the role of this facility may be changing. If so, this item will be reassessed as appropriate.</li> </ul>	Med
Timberline Lodge and Wy'est Day Lodge	Near Government Camp	<ul style="list-style-type: none"> <li>Provide information and orientation to the Mt. Hood south slope</li> <li>Provide historical information and interpretation regarding the lodge</li> </ul>	<ul style="list-style-type: none"> <li>Staffed reception area</li> <li>Map of forest</li> <li>Exhibits about the cultural history of the site</li> <li>Restrooms and drinking fountains</li> <li>Lodging</li> <li>Dining</li> <li>Skiing/winter sports</li> </ul>	<ul style="list-style-type: none"> <li>Develop exhibit plan for interior exhibits (Timberline Lodge and Wy'est Day Lodge)</li> <li>Redesign Forest Service reception area</li> <li>Create new audio-visual presentation</li> <li>Multi-panel exterior kiosk-working in conjunction with current planning efforts to provide information on:                             <ul style="list-style-type: none"> <li>Site orientation</li> <li>Recreational opportunities</li> <li>Safety and land use ethics</li> </ul> </li> </ul>	Med
Hood River Fruit Heritage Museum	Pine Grove at the Fruit Company	<ul style="list-style-type: none"> <li>Preserve the story of the fruit industry and the people who built it</li> <li>Provide information and orientation to the Fruit Loop and Mt. Hood National Scenic Byway</li> </ul>	<ul style="list-style-type: none"> <li>Staffed museum and sales area</li> </ul>	<ul style="list-style-type: none"> <li>Provide wayfinding</li> <li>Provide exterior signage at the building</li> <li>A full scale museum is in the works and scheduled to open in 2012</li> </ul>	Med

**Tertiary Information Centers (continued)**

Site	Location	Purpose/Objective	Existing Media/Facilities	Recommended Media/Facilities	Priority
TBD	Sandy	<ul style="list-style-type: none"> <li>• Provide information and orientation to the Byway</li> <li>• Highlight historic events in the Sandy area including its role on the Barlow Road</li> </ul>	<ul style="list-style-type: none"> <li>• Pioneer and logging history</li> <li>• Restrooms and drinking fountain</li> </ul>	<ul style="list-style-type: none"> <li>• Add approach signs</li> <li>• Add kiosk in new plaza highlighting Sandy as the Gateway to Mt. Hood</li> <li>• 4 - 36" x 48" x 1/8" high pressure laminate signs                             <ul style="list-style-type: none"> <li>• Orientation map of Byway</li> <li>• Commercial opportunities (dining, lodging, shopping, agriculture, outfitters, skiing, etc.)</li> </ul> </li> <li>• Highlight Clackamas County/Mt. Hood Territory significance and unique opportunities including Christmas Tree Capital, U-Pick-Farms, etc.</li> <li>• Mt. Hood Territory TeleTales installment (<a href="http://www.mthoodterritory.com/teletales.jsp">www.mthoodterritory.com/teletales.jsp</a>)</li> </ul>	Med
Hood River County Historical Museum	Hood River	Preserve the stories of the Hood River and Mid-Columbia River	<ul style="list-style-type: none"> <li>• Exhibits showing Native American life, early pioneers, the fruit industry history, windsurfing, and more</li> <li>• Exhibit honoring Japanese-Americans and their struggle during World War II</li> <li>• Restroom and drinking fountain</li> </ul>	<ul style="list-style-type: none"> <li>• Possible relocation site for the Hood River Chamber of Commerce</li> <li>• Highlight the story of how produce gets to the market</li> </ul>	Low

**Additional Planning**

Zigzag and Hood River Ranger Stations will require architectural design and exhibit planning for their new lobby areas. Timberline Lodge, Forest Service interior exhibits, and information desk, will also require an exhibit plan that details the stories and exhibits.

## Interpretive Sites Traveling from Troutdale to Hood River

### Multnomah and Clackamas County Roads

Milepost numbers denote travel from west to east.

Site	Location	Theme(s) and Storyline(s)	Existing Media/Facilities	Recommended Media/Facilities	Priority
West Entrance Portal - McMenamins Edgefield	2126 S.W. Halsey St., Troutdale	1-3 highlighting 2. K 3. F, G	Restaurants, micro-brewery, winery, organic gardens, vineyards, pub course, lodging, artwork, spa, and movie theatre	<ul style="list-style-type: none"> <li>Possible Entrance Portal; if not chosen, then should still be a point of interest</li> <li>Install approach signs</li> </ul>	High
Dodge Park	Milepost 15.4	1. B, C, D, F 2. C, G, H	<ul style="list-style-type: none"> <li>3-panel kiosk – highlights the Sandy River Gorge; National Wild and Scenic River and State Scenic River Waterway; and “A Bridge’s Life Story” (located on bridge outside of park)</li> <li>Historic restrooms, fountain, and recreational facilities</li> </ul>	<ul style="list-style-type: none"> <li>Install approach signs</li> <li>Move “A Bridge’s Life Story” to Dodge Park</li> <li>Expand interpretation to include information on Bull Run (Portland’s watershed); Stream and Fishery Restoration; and the Inter-urban trolley line (and future bike path)</li> </ul> <p><i>Note: Under pending route modification, this site would no longer be on the Byway. If so, these features could be added elsewhere or eliminated completely.</i></p>	Med

Site	Location	Theme(s) and Storyline(s)	Existing Media/Facilities	Recommended Media/Facilities	Priority
Oxbow Regional Park <i>(possible alternative to Dodge Park, pending route modification)</i>	Oxbow Parkway and Hosner Rd.	1. B, C, D, F 2. C, H 3. D	<ul style="list-style-type: none"> <li>Developed regional county park with camping, recreation sites, water access, restrooms, drinking water</li> <li>Opportunities for swimming, fishing, rafting and kayaking, pedestrian, biking and equestrian trails, and wildlife viewing</li> <li>Park naturalists provide nature education such as mushroom and wildflower identification, bird walks and animal tracking</li> </ul>	Maintain facilities and current nature education and services through Metro Regional Parks and Green spaces.	Med
J. Frank Schmidt Jr. Arboretum <i>(pending route modification and partnership agreement with owners)</i>	9500 SE 327th Ave., Boring	1. B 2. M 3. F	<ul style="list-style-type: none"> <li>Home to more than 800 species and cultivators of deciduous trees and shrubs</li> <li>Established in 1984 with additions made yearly as a nursery showroom</li> <li>Open to wholesale customers for viewing</li> </ul>	If Clackamas County owns the gravel surface parking area on the corner, include a kiosk interpreting the nursery landscape story. Include message that Arboretum is not open for public access	Low to Med
Jonsrud Viewpoint	Milepost 23.0	2. B, C, D	<ul style="list-style-type: none"> <li>3-wayside exhibits – First House West of the Cascades; The Devil’s Backbone; Sandy River Crossing</li> <li>Drinking fountain</li> </ul>	<ul style="list-style-type: none"> <li>Excellent views of Mt. Hood</li> <li>Provide wayfinding to site</li> <li>Redesign and fabricate existing signs using high pressure laminate</li> </ul>	Low

**US Highway 26 Sites**

*Milepost numbers denote travel from west to east.*

Site	Location	Theme(s) and Storyline(s)	Existing Media/ Facilities	Recommended Media/ Facilities	Priority
Sandy Area Historical Museum	Milepost 24.5	2. A, B, C, D, L, M 3. C, D, E, F, G, H	<ul style="list-style-type: none"> <li>Exhibit regarding emigrant settlement and logging</li> <li>Restrooms and Drinking Fountain</li> </ul>	<ul style="list-style-type: none"> <li>Secondary Entrance Portal (see above)</li> </ul>	High
Barlow Trail BLM/ County Park Historical Waysides and Recreation Area	Milepost 36.1	1. B, C, D, F 2. C, D, F, G 3. A, D, H	Construction of waysides and trail system begins in 2009	<ul style="list-style-type: none"> <li>Barlow Trail interpretive waysides and multi-recreation access, Sandy River Basin, “Backcountry” trail system, interpretive walks</li> <li>Construction of waysides and trail system 2009-2012</li> <li>Restrooms</li> <li>3 2-panel kiosks with maps, “Know Before You Go” safety information, Barlow Trail story of this site, stream/forest ecology</li> </ul>	High

Site	Location	Theme(s) and Storyline(s)	Existing Media/ Facilities	Recommended Media/ Facilities	Priority
Wildwood Recreation Site – Cascade Streamwatch	Milepost 39.3	1. B, C, D, F 2. N 3. E	<ul style="list-style-type: none"> <li>Developed recreation site with restrooms and drinking fountains</li> <li>Fisheries interpretation is extensive, including a underwater cross-section view of the stream</li> </ul>	<ul style="list-style-type: none"> <li>Install approach signs</li> <li>Eastbound right turn lane may be needed in the future</li> <li>Consider adding interpretive kiosk to highlight the site’s importance as a fishery and the commercial/hospitality offerings of the nearby villages. Site the kiosk on the highway side of the entrance gate so it is accessible to visitors year-round and doesn’t require payment to access. Consider expanded parking and cutting back of foliage for improved day-lighting and visibility from road.</li> </ul>	High
Mt. Hood National Forest Entrance Sign	Milepost 42.0	N/A	Historic National Forest sign	Maintain for its historic value	N/A
Zigzag Ranger Station	Milepost 42.0	1-3	Kiosk, staffed reception, restrooms and drinking fountain	<ul style="list-style-type: none"> <li>Secondary entrance portal replacing Mt. Hood Visitor Information Center</li> <li>Need to investigate need for turn refuge or improvement of sight distance</li> <li>Driveway improvements and eastbound right turn lane may be needed in the future.</li> </ul>	High

Site	Location	Theme(s) and Storyline(s)	Existing Media/ Facilities	Recommended Media/ Facilities	Priority
Arlie Mitchell Road, Marker and Trailhead	Milepost 43.9	2. D	Historic bronze marker placed in 1916	<ul style="list-style-type: none"> <li>• Site of a major stream crossing. Provide a 24" x 36" low profile interpretive sign</li> <li>• Provide better wayfinding signage for the Barlow Trail.</li> </ul>	Low
Barlow Road (Pioneer Bridle Trailhead)	Milepost 45.2	2. D, E, F, G	Historic tollgate replica, interpretive signs, trailhead signage and two benches	<ul style="list-style-type: none"> <li>• Consolidate the signage into a 2-panel kiosk</li> <li>• Provide map that shows the Barlow trail, current and historic Mt. Hood Loop Hwy.</li> <li>• Ensure that improvements retain adequate room for vehicle turn around on the Forest Service Road.</li> </ul>	Med
Little Zigzag Falls Trailhead	Milepost 48.6	2. F 3. C, D, E	Trailhead signage	Investigate feasibility of constructing parking and trailhead for Laurel Hill Historic Site at end of Forest Road 2639. Explore possibility of linking this effort to addition of pavement by ODOT to existing pull-out on Hwy. 26 eastbound between mile marker 47 and 48.5.	Med
Historic Mt. Hood Loop Highway Trail	Milepost 48.6	2. E, F, G	Trailhead signage	Investigate feasibility of constructing parking and trailhead for Laurel Hill Historic Site at end of Forest Road 2639. Explore possibility of linking this effort to addition of pavement by ODOT to existing pull-out on Hwy. 26 eastbound between mile marker 47 and 48.5.	Med

Site	Location	Theme(s) and Storyline(s)	Existing Media/ Facilities	Recommended Media/ Facilities	Priority
Laurel Hill Historic Marker and Trailhead	Milepost 50.9	2. D	<ul style="list-style-type: none"> <li>Historical marker</li> <li>Trailhead signage</li> </ul>	Need Forest and ODOT input to investigate relocating marker, trailhead, and turnout to location with better sight distance and expanded area for vehicle pull-out/parking. Investigate need for turn lanes	Med
Mirror Lake Trailhead	Milepost 51.8	2. H 3. H	Trailhead signage	Need Forest and ODOT input. (Note: ODOT does not support interpretive signage at current trailhead location due to poor safety history. They are investigating the option of relocating the trailhead and connecting trail to Ski Bowl.)	Med
Mt. Hood Cultural Center and Museum	Govt. Camp Loop	1. A, B, C, D, F 2. A, B, C, D, E, F, G, J 3. A, B, C, D, E, G, H	<ul style="list-style-type: none"> <li>Staffed facility, exhibits, art galleries and displays</li> <li>Restrooms and drinking fountain</li> <li>Mt. Hood Territory TeleTales installment (<a href="http://www.mthoodterritory.com/teletales.jsp">www.mthoodterritory.com/teletales.jsp</a>)</li> </ul>	<ul style="list-style-type: none"> <li>Add approach signs and sign facility</li> <li>Tertiary information center for the Byway</li> </ul>	High
National Historic Oregon Trail and Barlow Road Interpretive Site	Milepost 53.9	2. D, E	Shelter with interpretive signs and picnic table	Re-fabricate signs using high pressure laminate	Med

**MT. HOOD NATIONAL SCENIC BYWAY INTERPRETIVE PLAN & DESIGN GUIDELINES**

Site	Location	Theme(s) and Storyline(s)	Existing Media/ Facilities	Recommended Media/ Facilities	Priority
Timberline Lodge Road Junction and Sign	Milepost 54.2	N/A	Timberline Lodge site sign	Construct new landmark sign	High
Timberline Lodge	Timberline Road	1. A, B, C, 2. J 3. A, B, C, D, E, H	Restrooms and fountain Exhibits, movie on the construction of lodge	Tertiary Information Center (see above)	Med
Summit Meadow (Barlow Road Trailhead)	Milepost 0.4 off Forest Road 2650	2.D, H	Campground, 3-panel kiosk (empty), and trail signs	Install approach signs	Low
Summit Meadow (Interpretive Site)	Milepost 0.4 off Forest Road 2650	2. A, B, D 3. E	<ul style="list-style-type: none"> <li>Interpretive sign, historical marker, and bench</li> <li>Pioneer cemetery</li> <li>Mt. Hood Territory TeleTales installment (<a href="http://www.mthoodterritory.com/teletales.jsp">www.mthoodterritory.com/teletales.jsp</a>)</li> </ul>	No additional media recommended	Low

**Oregon Highway 35 Sites**

*Milepost numbers denote travel from west to east.*

Site	Location	Theme(s) and Storyline(s)	Existing Media/Facilities	Recommended Media/Facilities	Priority
<p>Forest Road 2531 (Old Loop Highway)</p> <p>This section provides an alternative route – an opportunity to get closer to the resources</p>	Milepost 58.0 and 60.0	2. D, E 3. D, E	<ul style="list-style-type: none"> <li>• Parking, Trailhead and Historic Sites</li> <li>• Pioneer Women’s Grave</li> <li>• Historic roadside fountain</li> <li>• Barlow Pass Trailhead</li> <li>• Drivable section of historic Mt. Hood Loop Hwy.</li> <li>• Pacific Crest Trailhead and Twin Lakes Trailhead</li> <li>• Sno-Park access</li> </ul>	<ul style="list-style-type: none"> <li>• Provide a 3- panel kiosk (36”x 48” high pressure laminate panels) with the following information:                             <ul style="list-style-type: none"> <li>• Orientation map to road section</li> <li>• Brief overview of attractions along section</li> <li>• Hiking trail information</li> <li>• Safety and ethics</li> </ul> </li> <li>• Investigate need for turn refuge or improvement of sight distance. Consider winter closure due to low usage and difficulty of exit turns as a result of snow banks.</li> </ul>	Med
Pioneer Woman’s Grave	Milepost 58.0 and 60.0	2. D	Gravesite, bronze plaque and interpretive sign	<ul style="list-style-type: none"> <li>• Maintain for its historic value</li> <li>• Investigate need for left and right turn refuges and sight distance improvements. Consider winter closure due to low usage and difficulty of exit turns as a result of snow banks.</li> </ul>	Med
Historic Fountain	Milepost 58.0 and 60.0	2. D, E	Historic fountain (1 of 3 on the Byway and the only one in working order)	Provide information regarding fountain at kiosk	Med
Barlow Pass Trailhead and Sno-Park	Milepost 58.0 and 60.0	2.D, E	<ul style="list-style-type: none"> <li>• Parking and Trailhead</li> <li>• Trailhead map (in disrepair)</li> <li>• Interpretive signs- Barlow Pass trail ruts</li> </ul>	Replace existing sign with new kiosk	Med

Site	Location	Theme(s) and Storyline(s)	Existing Media/Facilities	Recommended Media/Facilities	Priority
Barlow Road Junction	Milepost 58.0 and 60.0	N/A	Rustic directional sign for the Barlow Road	Maintain for historic significance	N/A
Barlow Road to Devil's Half Acre Meadow	Milepost 58.0 and 60.0	3. E	None	Provide information regarding site at kiosk	Med
White River Crossing	Milepost 61.8	1. A, B, C, E 3. C	<ul style="list-style-type: none"> <li>Restroom</li> <li>Parking area is unstable and subject to debris flows</li> </ul>	<ul style="list-style-type: none"> <li>Excellent example of how the mountain is dynamic. Provide a 2 panel kiosk (36"x 48" high pressure laminate) to include:                             <ul style="list-style-type: none"> <li>Arial map/photos of mountain before and after 2007 debris flow</li> <li>Narrative on the mountain's geological processes and cause for debris flow</li> </ul> </li> <li>Area currently being studied by geologists and others. Plans for site should be considered tentative pending findings. Due to instability of site, any interpretation investments should be carefully and creatively shaped. Coordinate any efforts at site with FHWA-WFLHD.</li> <li>Investigate need for turn refuge or improvement of sight distance.</li> <li>Locate kiosk in the west end of the parking lot</li> </ul>	Med

Site	Location	Theme(s) and Storyline(s)	Existing Media/Facilities	Recommended Media/Facilities	Priority
<p>Forest Road 3445 (Sahalie Falls Loop)</p> <p>Alternative route to Hwy. 35 (another opportunity for visitors to slow down and enjoy the area)</p>	Milepost 64.0	<p>1. B</p> <p>2. D, E, F, G, N</p> <p>3. D, E</p>	<ul style="list-style-type: none"> <li>1.7 mile drivable section of the Historic Mt. Hood Loop Hwy.</li> <li>Access point to Sahalie Falls Viewpoint</li> <li>Sahalie Falls Bridge (or South Fork Hood River Bridge)</li> <li>Historic fountain</li> </ul>	<ul style="list-style-type: none"> <li>Provide a 3-panel kiosk (36" x 48" high pressure laminate panels) to include:                             <ul style="list-style-type: none"> <li>Orientation map to road section</li> <li>Brief overview of attractions along section</li> <li>Narrative on bridge work</li> <li>Hiking trail information</li> <li>Safety and land use ethics</li> </ul> </li> <li>Place kiosk on the Historic Mt. Hood Loop Hwy. rather than US 26.</li> <li>Bridge is being upgraded to allow vehicle traffic</li> </ul>	Med
Tamanawas Falls Trailhead	Milepost 72.3	<p>1. A, B, F</p> <p>3. D, E, H</p>	<ul style="list-style-type: none"> <li>One of the most used trails on the Mt. Hood National Forest</li> <li>Parking and interpretive signs randomly placed</li> </ul>	<ul style="list-style-type: none"> <li>Reconfigure the site to improve parking and add restroom</li> <li>Install 3-panel kiosk (36"x48" high pressure laminate) replacing existing interpretive signs. Include:                             <ul style="list-style-type: none"> <li>Map of trail to Tamanawas Falls</li> <li>Safety and ethics</li> <li>Interpretive signs about the East Fork Hood River restoration project</li> <li>Habitat improvements for fisheries</li> </ul> </li> <li>Investigate need for right and left turn refuges and sight distance improvements</li> </ul>	Med

**MT. HOOD NATIONAL SCENIC BYWAY INTERPRETIVE PLAN & DESIGN GUIDELINES**

Site	Location	Theme(s) and Storyline(s)	Existing Media/Facilities	Recommended Media/Facilities	Priority
Parkdale – Hudson Museum and Parkdale – Mt. Hood Terminus (Railroad)	Baseline Road near milepost 85.0	1. F 3. D, F, G	<ul style="list-style-type: none"> <li>Museum with interior and exterior exhibits, historical buildings and farming equipment, restrooms, picnic area and southern terminus for Historic Scenic Mt. Hood Railroad</li> <li>3 Forest Service interpretive signs</li> </ul>	<ul style="list-style-type: none"> <li>Improve wayfinding</li> <li>Forest Service signs don't fit the site and are in need of redevelopment</li> <li>Add signage regarding history of the train</li> <li>Interpretive signage should be placed on Baseline Road outside of the US 26 intersection area due to poor accident history and lack of left turn lane.</li> </ul>	Med
Hood River Ranger Station	Milepost	1-3	Kiosk, staffed reception, restrooms and drinking fountain	<ul style="list-style-type: none"> <li>Secondary entrance portal</li> <li>Investigate need for right and left turn refuges and sight distance improvements.</li> </ul>	High
Mt. Hood Towne Hall  Southern Entrance Point for the Fruit Loop Tour  Use Fruit Loop as alternative route to Hwy 35	Milepost 85.1 at OR Hwy 281	1-3	<ul style="list-style-type: none"> <li>Historic school house (currently used as a community center)</li> <li>Historical marker</li> </ul>	<ul style="list-style-type: none"> <li>Provide 3 panel kiosk (36" x 48" high pressure laminate) to include:                             <ul style="list-style-type: none"> <li>Introduction and orientation map to the Fruit Loop</li> <li>Brief history of farming in the Hood River area (refer visitors to the Hood River Historical Museum)</li> <li>Commercial opportunities (lodging, restaurants, attractions, etc.)</li> </ul> </li> <li>Investigate need for right and left turn refuges and sight distance improvements</li> </ul>	Med

Site	Location	Theme(s) and Storyline(s)	Existing Media/Facilities	Recommended Media/Facilities	Priority
Gilhouley-Miller Roads Intersection Viewpoint	Milepost 90.4	1. A, B, E 2. B, H, L 3. D, E, F, G, H	None	<ul style="list-style-type: none"> <li>Provide 3-panel kiosk (36" x 48" high pressure laminate) to include                             <ul style="list-style-type: none"> <li>Trailhead/Orientation map</li> <li>Know Before You Go/Safety and land use ethics</li> <li>Logging in the Hood River (County Forest)</li> </ul> </li> <li>Site requires extensive work to accommodate proposed facilities listed above</li> <li>Investigate need for right and left turn refuges and sight distance improvements</li> </ul>	Med
Pine Grove	Milepost 97.8	1. B, E 3. F, G	<ul style="list-style-type: none"> <li>Site of the Hood River County Fruit Heritage Museum located within The Fruit Company facility.</li> <li>Outstanding exhibits highlighting the history of the fruit industry within the valley and illustrating the types of conditions workers lived and worked in.</li> </ul>	<ul style="list-style-type: none"> <li>Provide wayfinding</li> <li>Fruit Heritage Museum is a Tertiary Information Center</li> </ul>	Med
Mt. Hood Railroad Depot	1 <sup>st</sup> Street and Cascade Ave.	1.B, E 2. A, B, C, G, L 3. D, E, F, G	<ul style="list-style-type: none"> <li>Parking, interpretive signs, bronze plaque, restrooms, picnic tables</li> <li>North terminus for the Historic Mt. Hood Railroad</li> </ul>	<ul style="list-style-type: none"> <li>Provide wayfinding</li> <li>Possibility for living history programs</li> </ul>	Med

Site	Location	Theme(s) and Storyline(s)	Existing Media/Facilities	Recommended Media/Facilities	Priority
Hood River County Historical Museum and Hood River Pedestrian Bridge	East Port Marine Dr. (Port of Hood River Marina)	<ol style="list-style-type: none"> <li>1. A, B, F, H,</li> <li>2. A, B, C, G, L</li> <li>3. C, D, E, F, G</li> </ol>	<ul style="list-style-type: none"> <li>• Parking, museum, interpretive signs, restrooms, drinking water, picnic tables</li> <li>• Proposed site for the new Hood River County Visitor Information Center and Chamber of Commerce</li> <li>• Existing interpretation is focused on the Columbia River (CRGNSA)</li> </ul>	<ul style="list-style-type: none"> <li>• Provide wayfinding</li> <li>• Good location to tell the story of how produce gets to the market</li> </ul>	Low

## NOTES

## ADDITIONAL MEDIA RECOMMENDATIONS

### Byway-wide

Currently there are several websites that highlight the Byway, including the National Scenic Byway Program and ODOT. There are opportunities to improve these sites; however, a Mt. Hood National Scenic Byway website (managed by partners) is also a possibility.

The Byway also lends itself to webcast technology. Suggested webcasts include:

- **Cultural History** (one for each topic):  
Barlow Road, Timberline Lodge, Edgefield, Lewis and Clark, the legend of Wy'east, railroad/logging history, climbing Mt. Hood
- **Natural History** (one for each topic):  
Oregon Birding Trail, watershed restoration for salmon, creation of the Mt. Hood National Forest, and geology
- **Recreation Opportunities:** Winter sports, climbing, bicycling, water sports, hiking, sightseeing, fishing, camping, berry picking, Christmas tree cutting
- **Agritourism:** Fruit Loop, U-pick Farms, nursery stock, wineries, farmer markets,
- **Culinary tours:** Highlight the slow food movement in the area, interviews with local farmers and chefs, highlight restaurants
- **Seasonal Events:** Festivals, fairs, and other events

Logos could be developed for each emphasis area and used throughout various media to cue visitors. Long-term, a brochure could be developed that includes the logos. This brochure would consolidate many of the existing brochures and serve as an official publication to market the Byway. Currently, the available information is segmented, making it more difficult for the visitor to see the big picture.

### Hood River Side

The Hood River valley offers visitors an assortment of opportunities. Emphasis should be placed on promoting the new Hood River County Fruit Heritage Museum. This facility could serve as a launching point to encourage visitors to visit many of the farms, vineyards, and other attractions throughout the area.

### Multnomah County/Clackamas County Side

**Nurseries** - Create an opportunity for visitors to learn about this industry. Driving through the different nurseries generates a lot of questions that could serve as the foundation for a vibrant learning opportunity.

## NOTES

## DESIGN GUIDELINES

### Introduction

The Mt. Hood Scenic Byway is an extremely distinctive and diverse corridor that winds through dramatic landscapes, climates, and diverse cultural and historical influences. This heritage is celebrated to a great extent through the region's built environment. A majority of the area's exceptional design tradition relies on a combination of elements from Native Americans and European settlers (including the Steiner cabins), the rustic regional architectural style called Cascadian, the CCC's rustic



craftsmanship, the WPA's Arts and Crafts style, and Northwest Modernism. There are also numerous natural design influences that have helped shape the local built environment, including the massive form of Mt. Hood, immense trees, dramatic rock formations, abundant precipitation and variable climates.

Mt. Hood Scenic Byway is home to numerous communities that take pride in this beautiful area. It is highly valued by residents and visitors for both its scenic and recreational value and as an economic generator. Although this corridor has been a favorite route for over 100 years, the Byway currently lacks a cohesive identity. Signage, wayside structures, and portal design set the tone for the visitor experience and are important way-showing elements to seamlessly guide visitors through a corridor. While the Byway has many good examples of appropriate design, the current hodgepodge of design elements are generally confusing to the public and detract from the overall visitor experience.

The interpretive thematic framework outlined in this document is a strategic and cohesive set of narratives that are

closely tied to the ecological and cultural history of the place. Similarly, there must be a correlating system of facilities and structures that are cohesive and closely tied to the place. In order to create a cohesive and legible identity for the Mt. Hood Scenic Byway it must become a coherent pathway linked by various sites, venues and opportunities to tell the stories of the place.



**Purpose of Design Guidelines**

The guidelines aim to provide guidance for the physical design and related considerations for site planning of interpretive facilities. They seek to improve the image, aesthetics, sustainability, cohesiveness, and overall quality and visitor experience of the Mt. Hood Scenic Byway.

To achieve this aim, the guidelines:

- Incorporate principles of sustainability as an integral part of site planning and facility development.
- Describe a process that fits facilities within the context of the corridor’s ecological and cultural heritage.
- Facilitate an experience that encourages tourism and recreation in Multnomah, Clackamas and Hood River counties.
- Connect the built environment to the land and promote stewardship.
- Outline design principles that create harmony within this spectacular setting.
- Provide a broad range of facilities to meet visitor needs.
- Assist in the creation of a visual consistency and organization and a lasting sense of place.
- Provide way-showing for the visitor and provide the byway with a cohesive and unique character/identity.
- Enhance the intrinsic, unique qualities of the corridor.
- Describe elements that are appropriate to the diverse micro-climates and development densities of the byway.
- Maintain a bold architectural style that complements existing structures and indicates permanence, durability and enduring quality.

## Definition of Cascadian Architecture

from Timberline 1998 Environmental Assessment

This is a list of the character defining elements used to objectively define the style of architecture of Timberline Lodge. Its intent is to serve as a guide to determine design compatibility of new structures. This list of elements was developed by a group of professional architects and preservation specialists, and a representative of the State Historic Preservation office.



### Form

- low hugging profile
- steep roof pitches
- dormers and porch projections
- large chimneys

### Native Materials

- massive timbers and rock works
- varied exterior textures
- rough hewn wood and rock

### Details

- multi-paned windows
- hand-worked metal fixtures
- hand-crafted wood details
- window shutters

**Existing Character Images and Design Principles**

Inspiration for facility designs along the Mt. Hood Scenic Byway should be born from the landscape, its ecological and cultural history, and existing iconic architectural structures. Designs should reinforce to the casual visitor that they are passing along this special route. They should also be consistent with the US Forest Service Built Environment Image Guide (BEIG) and complement recent design elements (e.g. Government Camp signage and new Timberline Lodge kiosk). Designs should not merely repeat historical precedent, but instead, reveal a contemporary expression of respect for the rich design traditions of the Mt. Hood region and honor the particular uniqueness of the place.



**Materials**

- Facilities should have expressed structure of stone and heavy timber.
- Massive building materials should



complement the scale of the landscape features (i.e., large timbers in massive forests).

- Avoid overusing wood, especially scarce species or sizes in order to preserve old timber resources yet still achieve the Cascadian style. Consider clustering timber members together to increase massive expression.



- Assemble elements with simple, natural, not overly refined materials.



- Walls should have an appropriate, naturalized massing of stone elements that appear to be growing from the ground.



- Use strong, massive, battered stone bases on structures to complement the dramatic landscape of the Mt. Hood area. This reduces wear and maintenance, protects structures from the standing snow and evokes sturdiness and permanence.



- Use natural colored standing-seam metal roofs to blend with vegetation and reduce the need for maintenance.



- Use hammered iron for details and to join timbers.



### **Textures**

- Textures should complement the bold textures of the surrounding landscape.



- Use vertical board and batton wall textures to complement the strong verticality of the forests.



**Forms**

- Forms should be substantial and exhibit a sense of solidness.
- Materials should be bold and complement the landscape.



- Use gable-end entries on kiosks and restrooms with open gables to allow light to enter the structure.



- The roof should be designed so that it is fairly steep (8:12 -12:12) and dominates the architectural composition.

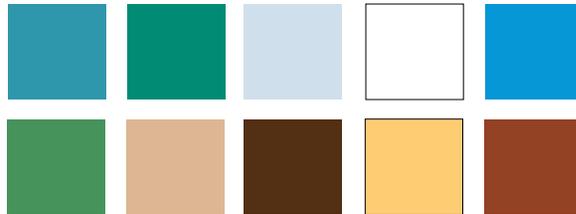
**Colors**

- Main colors should be muted earth tones and blend with and complement the surrounding landscape.
- Main colors should be used in the major structural elements of signs and kiosks (e.g. posts, beams, bases, metal work), roof materials and site elements (e.g. benches, trash receptacles).





- Accent colors should utilize the existing color palette from the Mt. Hood Scenic Byway logo and roadway sign.
- Accent colors should be used in sign faces, lettering, logos and to highlight special features and details.



**Site**

- Locate facilities sensitively and appropriately within the landscape to preserve sensitive resources and enhance its natural beauty.
- Locate facilities to be subordinate to landscape features.



- Place structures at the edge of existing clearings to preserve vegetation and views.
- Use native plant materials to provide interest, to create a relationship to the facility's natural surroundings, to support biodiversity and to reduce the need for maintenance.



- Place boulders in a natural arrangement to guide vehicular and pedestrian traffic.



***Private Character***

- Private businesses along the byway should consider following the recommendations outlined in these guidelines to help sustain the overall character of the corridor.

- Using local materials and architectural style helps soften the contrasting visual impact of private businesses.
- Design compatibility of architectural and landscape styles in private developments helps create a more cohesive and enjoyable visitor experience.



## Prototypical Structures

The following pages include the conceptual design development of prototypical structures. These are schematic designs meant to articulate the basic character and described approach to appropriate design along the Mt. Hood Scenic Byway. These prototypes represent specific applications of the design guidelines in order to address particular needs and identified interpretive sites along the byway. For specific locations refer to: Structure location map (pg. 46-47) and Existing and Proposed Facilities, Services, Interpretive Media Recommendations and Priorities (pg. 12).

### Gateway Markers:

Located at each Entrance Portal to give the visitor a sense of arrival on the byway.



### Community/City/Village Markers:

Located at each of the villages along the byway to identify the unique settlements, invite people to stop and add to a sense of place.



### Visitor Kiosk/Restroom:

Located at each Entrance Portal to orient visitors to the corridor and provide restroom and drinking fountains.



### Visitor Kiosks:

Located at primary sites along the byway. Roofed structures containing four interpretive panels.



**Secondary Visitor Kiosk:**

Located at secondary sites along the byway. Small structure containing two double-sided interpretive panels.

5



**Secondary Destination Sign:**

Located to orient vehicles to secondary destinations along the byway.

7



**Primary Destination Sign:**

Located to orient vehicles to primary destinations along the byway.

6



**Trail/Informational Marker:**

Located to orient and provide information to pedestrians at trailheads and waysides.

8



**National Forest Signs:**

Located at National Forest boundaries and facilities. Enhanced with battered stone bases.



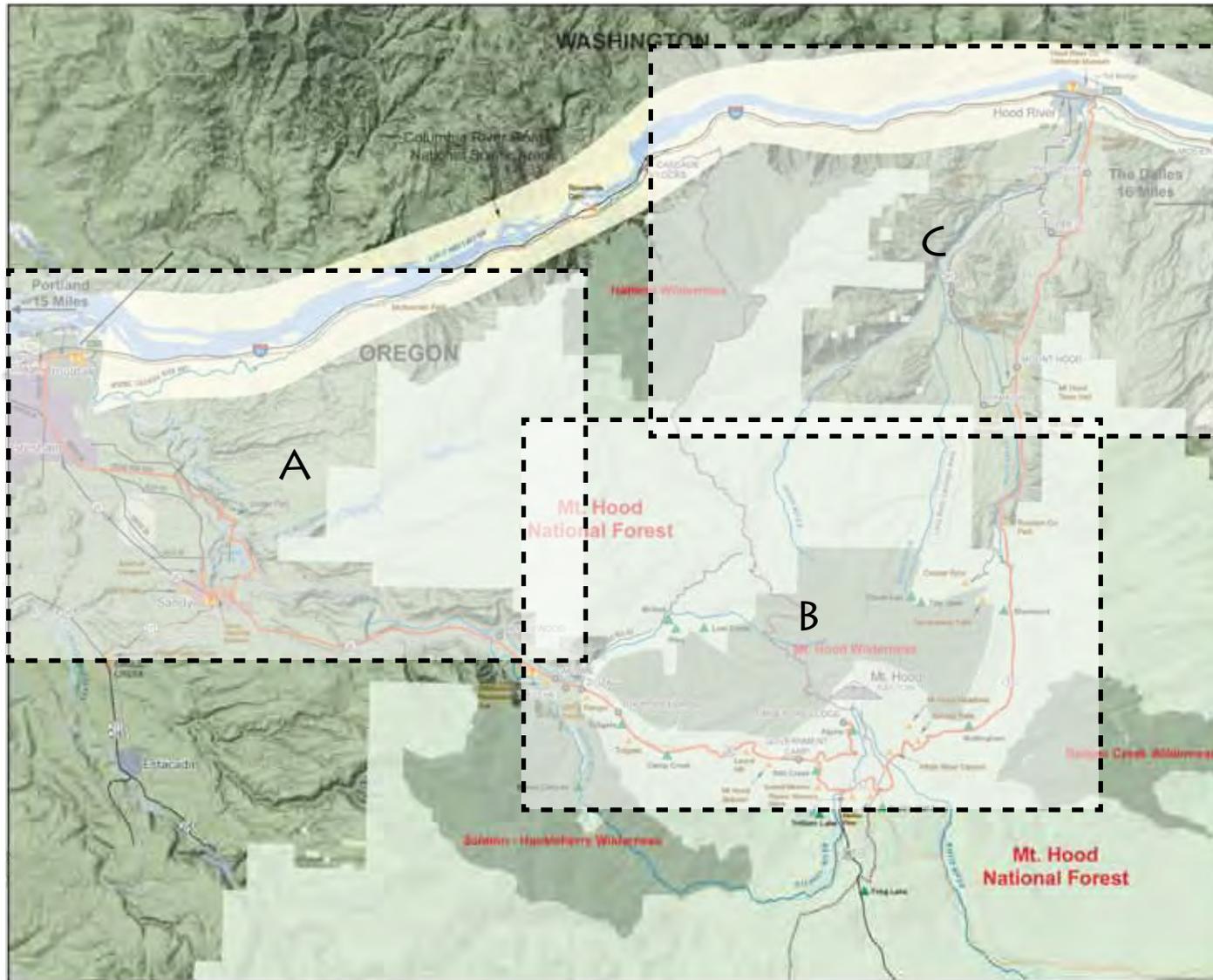
**Low Profile Exhibits:**

Located where needed along the byway. Single, standard interpretive panel.



\*All structures should be constructed of basic, local and easy to maintain materials, and be designed to deter vandalism.

**Structure Locations - Key Map**



\*Structure locations are approximate. Exact locations will need to be determined and field checked during site design.

- Mt. Hood Scenic Byway
- Historic Columbia River Highway/All-American Road
- Visitor Information Center
- Lewis and Clark Historic Trail
- Pacific Crest Trail
- The Oregon Trail
- Points of Interest
- ▲ Mt. Hood Campgrounds
- Toll Bridge
- Railroad



This map is based on information derived from Clatsop County's GIS.

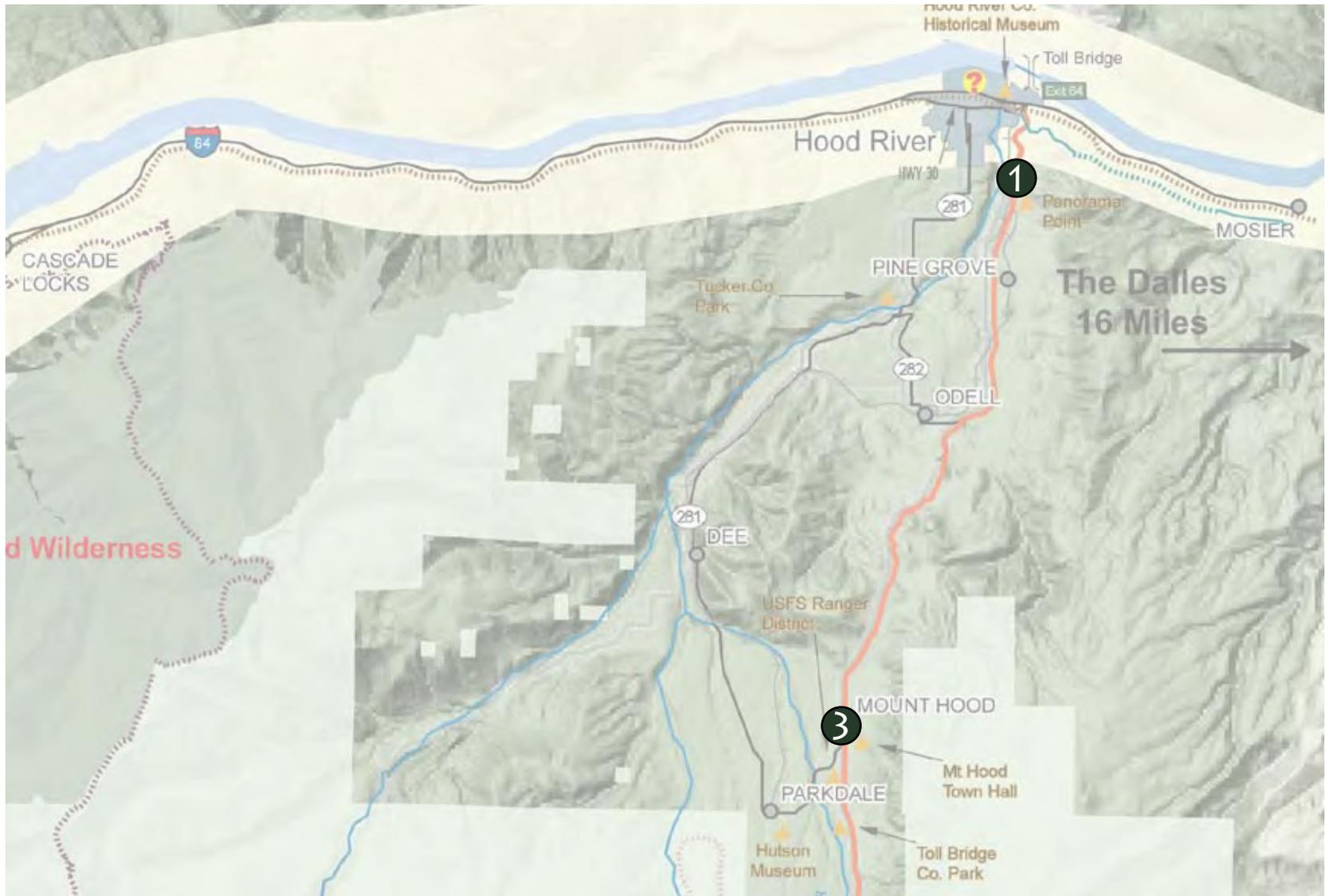
Map A



Map B



Map C



### Gateway Markers

\*Gateway Markers, as actually constructed, should consider a range of design details including conventional electrical hook-ups, internal solar generation capability, or no electricity at all, and could include glass panels in lanterns, panels of some other material, or no panels.



Metal standing seam roof

Consider solar power for lantern

Recommended Font: **LITHOS PRO**

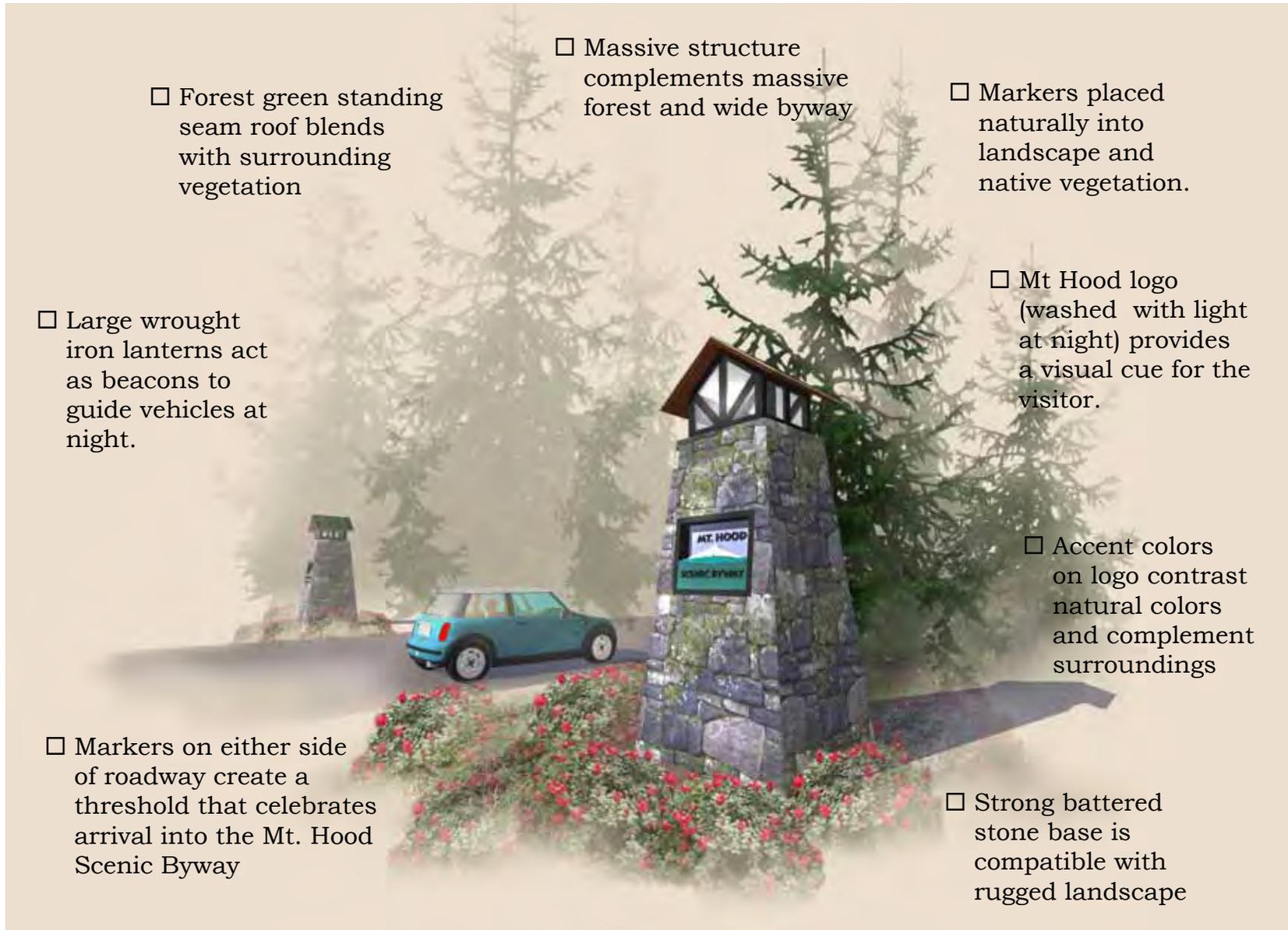
Hammered wrought iron framed lantern

Recessed, Lighted, powder-coated metal logo



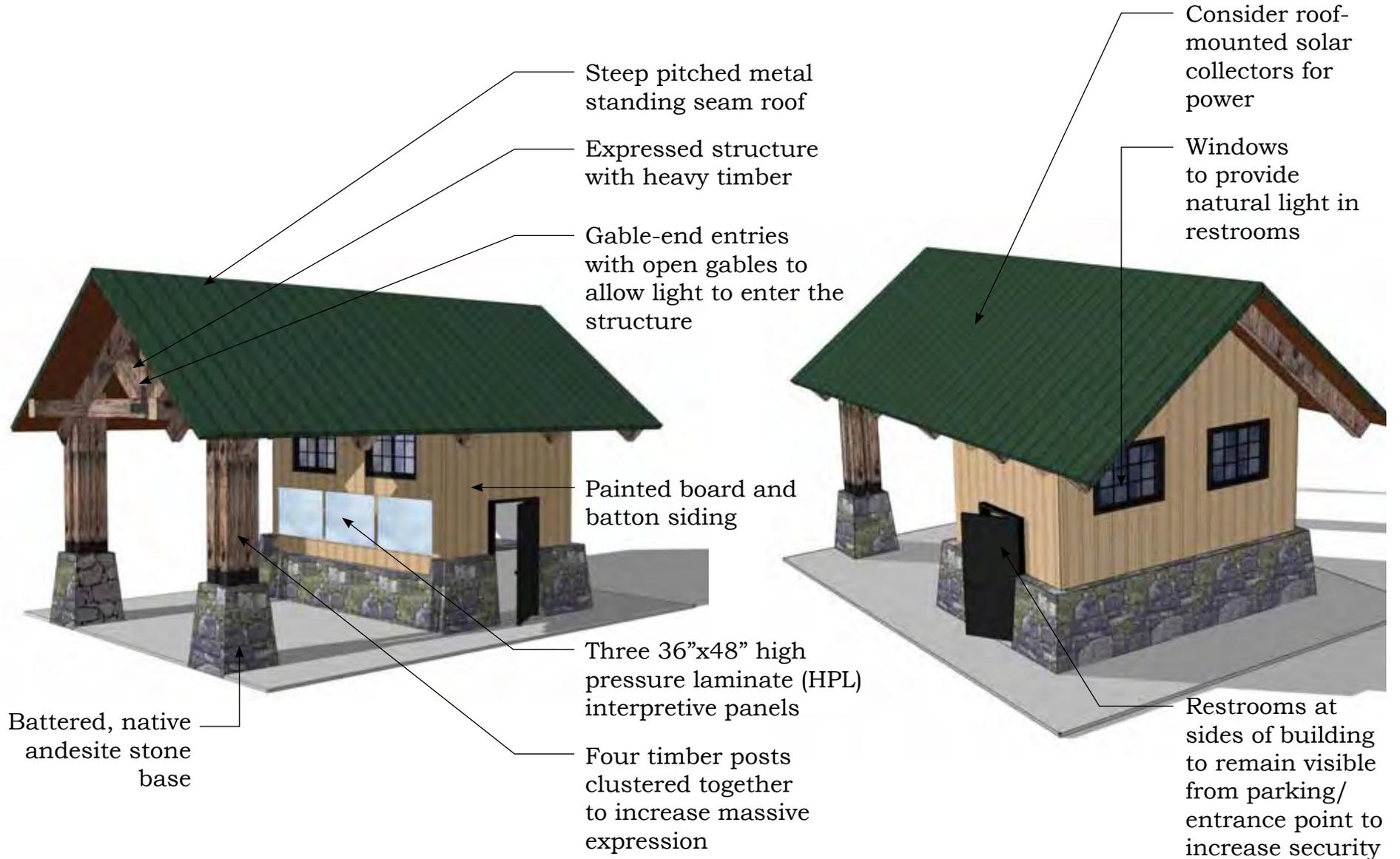
Battered, native andesite stone base

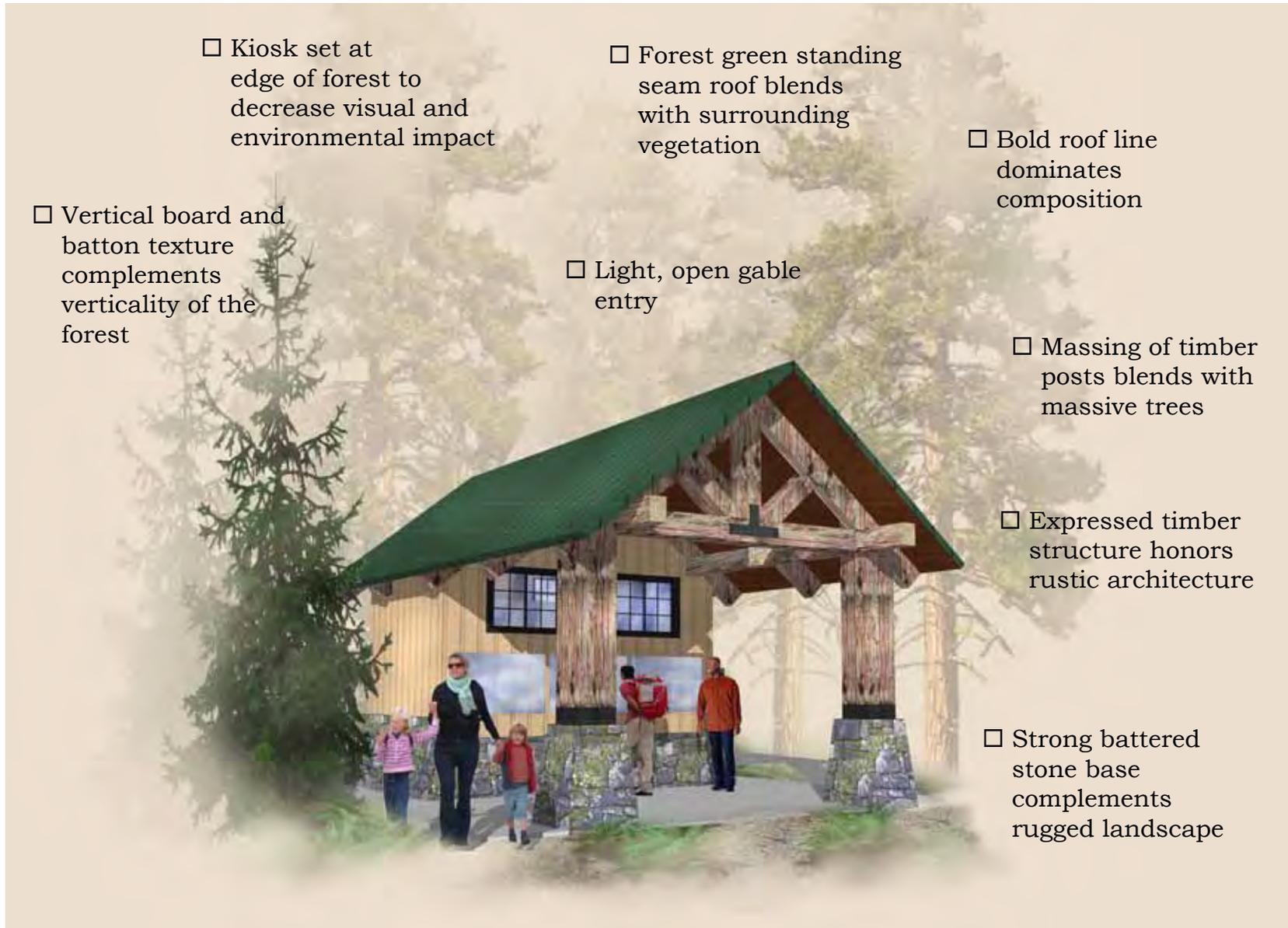
\*Gateway Markers should have appropriate setback from road or appropriate vehicle safety barriers according to ODOT standards.



- Forest green standing seam roof blends with surrounding vegetation
- Massive structure complements massive forest and wide byway
- Markers placed naturally into landscape and native vegetation.
- Mt Hood logo (washed with light at night) provides a visual cue for the visitor.
- Accent colors on logo contrast natural colors and complement surroundings
- Strong battered stone base is compatible with rugged landscape
- Large wrought iron lanterns act as beacons to guide vehicles at night.
- Markers on either side of roadway create a threshold that celebrates arrival into the Mt. Hood Scenic Byway

**Visitor Kiosk/Restroom**





□ Kiosk set at edge of forest to decrease visual and environmental impact

□ Forest green standing seam roof blends with surrounding vegetation

□ Bold roof line dominates composition

□ Vertical board and batton texture complements verticality of the forest

□ Light, open gable entry

□ Massing of timber posts blends with massive trees

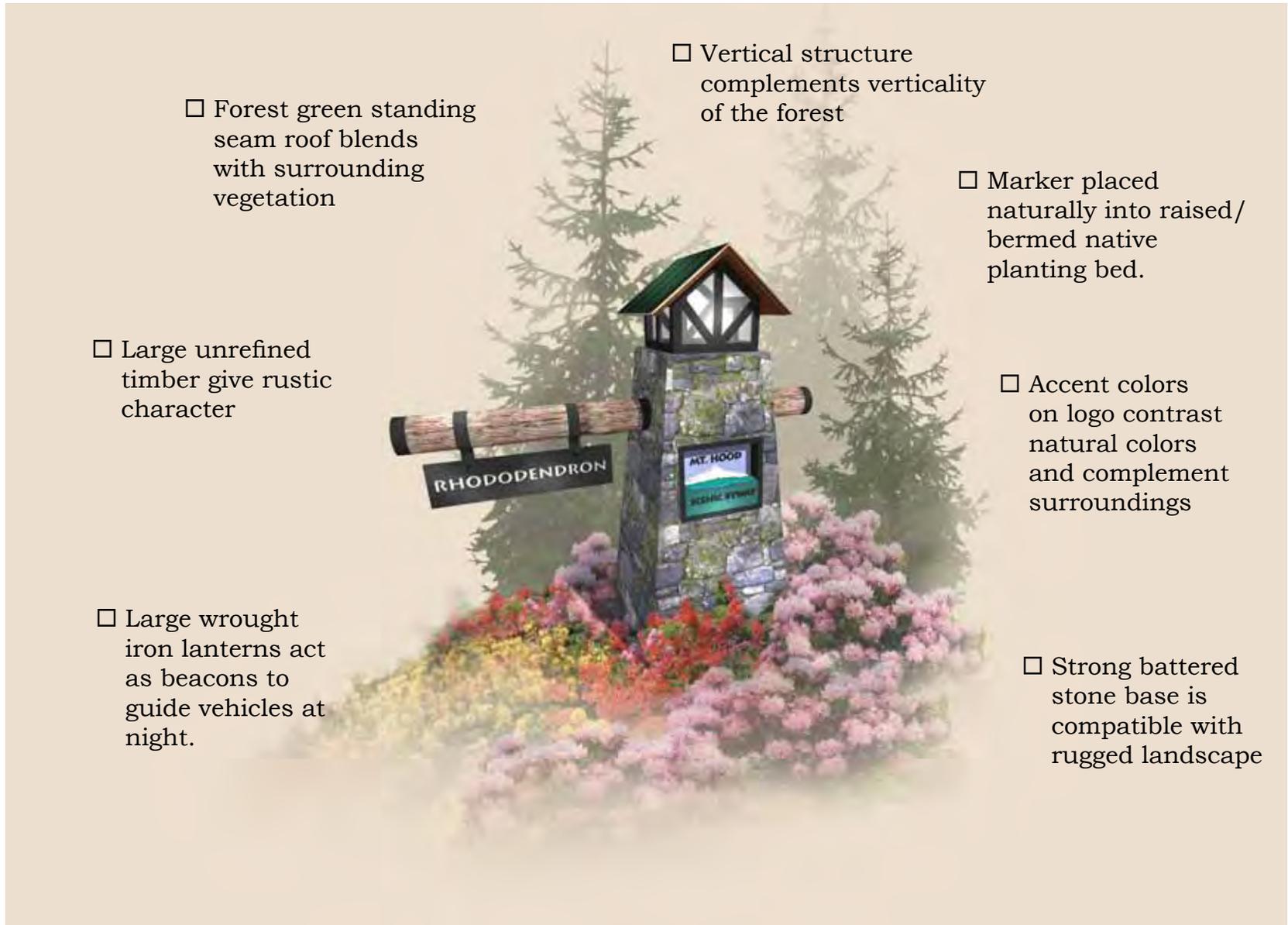
□ Exposed timber structure honors rustic architecture

□ Strong battered stone base complements rugged landscape

**Community/City/Village Marker**



\*Community/City/Village Markers should have appropriate setback from road or appropriate vehicle safety barriers according to ODOT standards.



□ Forest green standing seam roof blends with surrounding vegetation

□ Vertical structure complements verticality of the forest

□ Marker placed naturally into raised/bermed native planting bed.

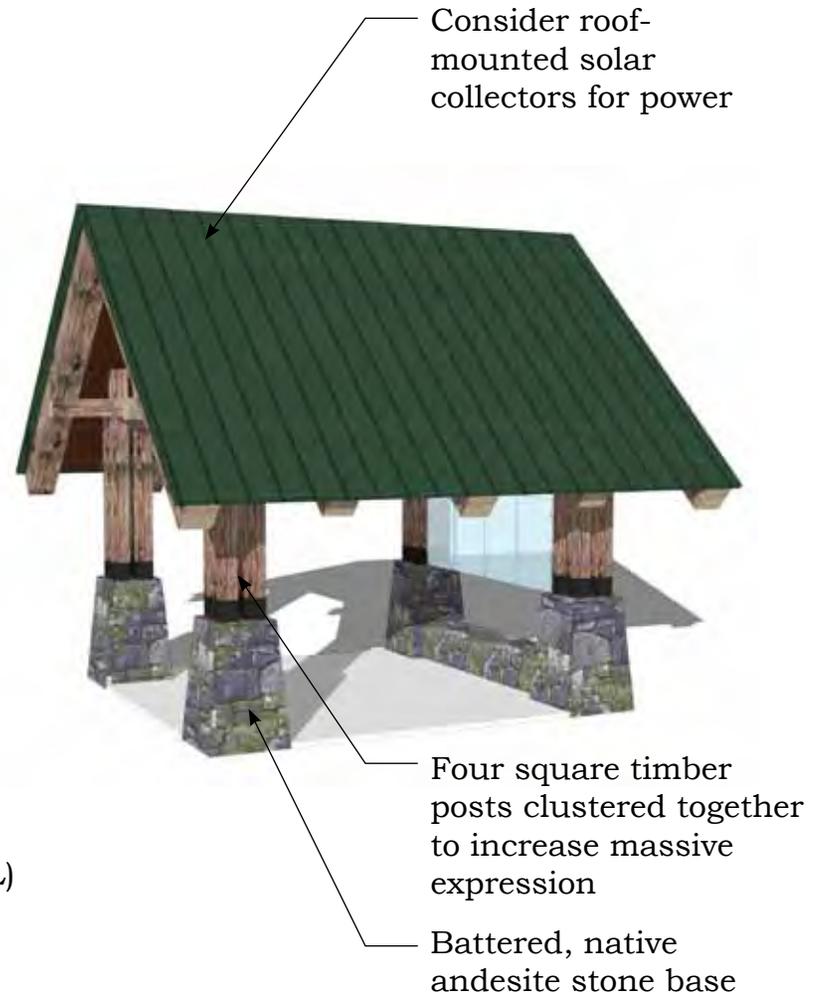
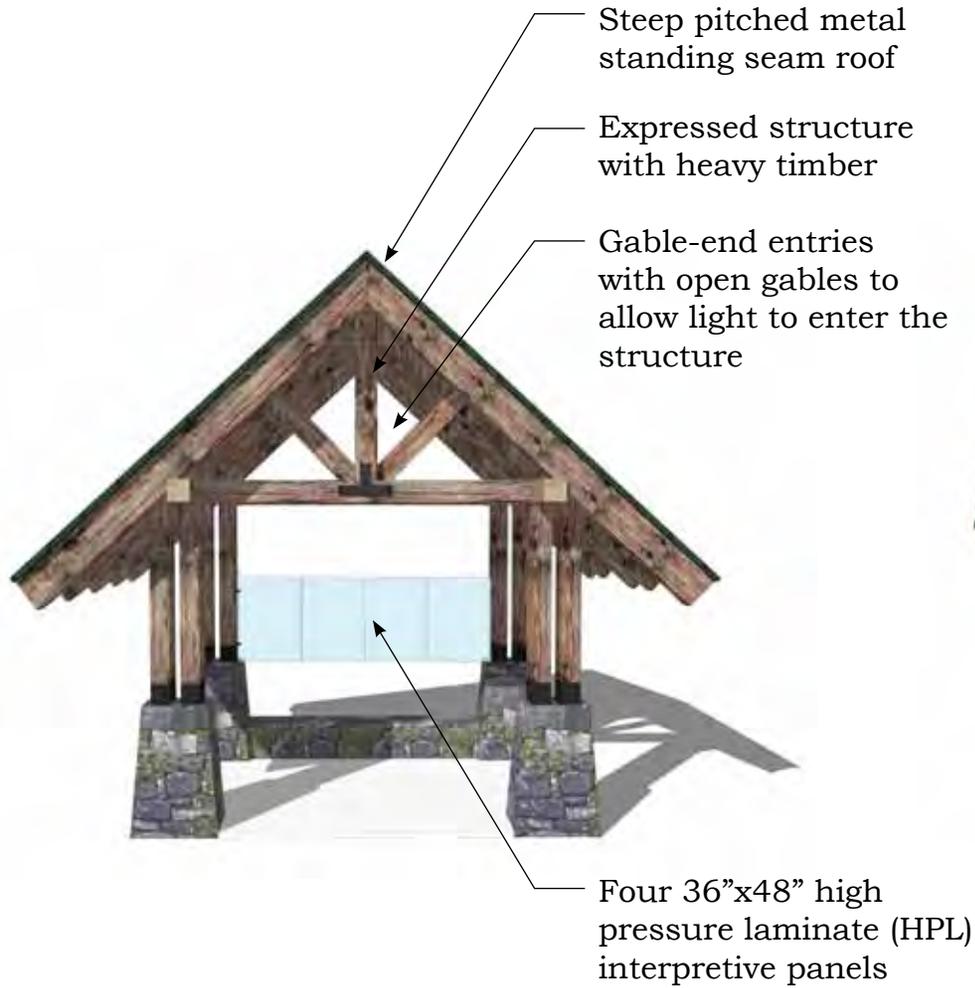
□ Large unrefined timber give rustic character

□ Accent colors on logo contrast natural colors and complement surroundings

□ Large wrought iron lanterns act as beacons to guide vehicles at night.

□ Strong battered stone base is compatible with rugged landscape

**Visitor Kiosk**





□ Structure is open and airy allowing views and connection to surrounding landscape

□ Kiosk set at edge of forest to decrease visual and environmental impact

□ Forest green standing seam roof blends with surrounding vegetation

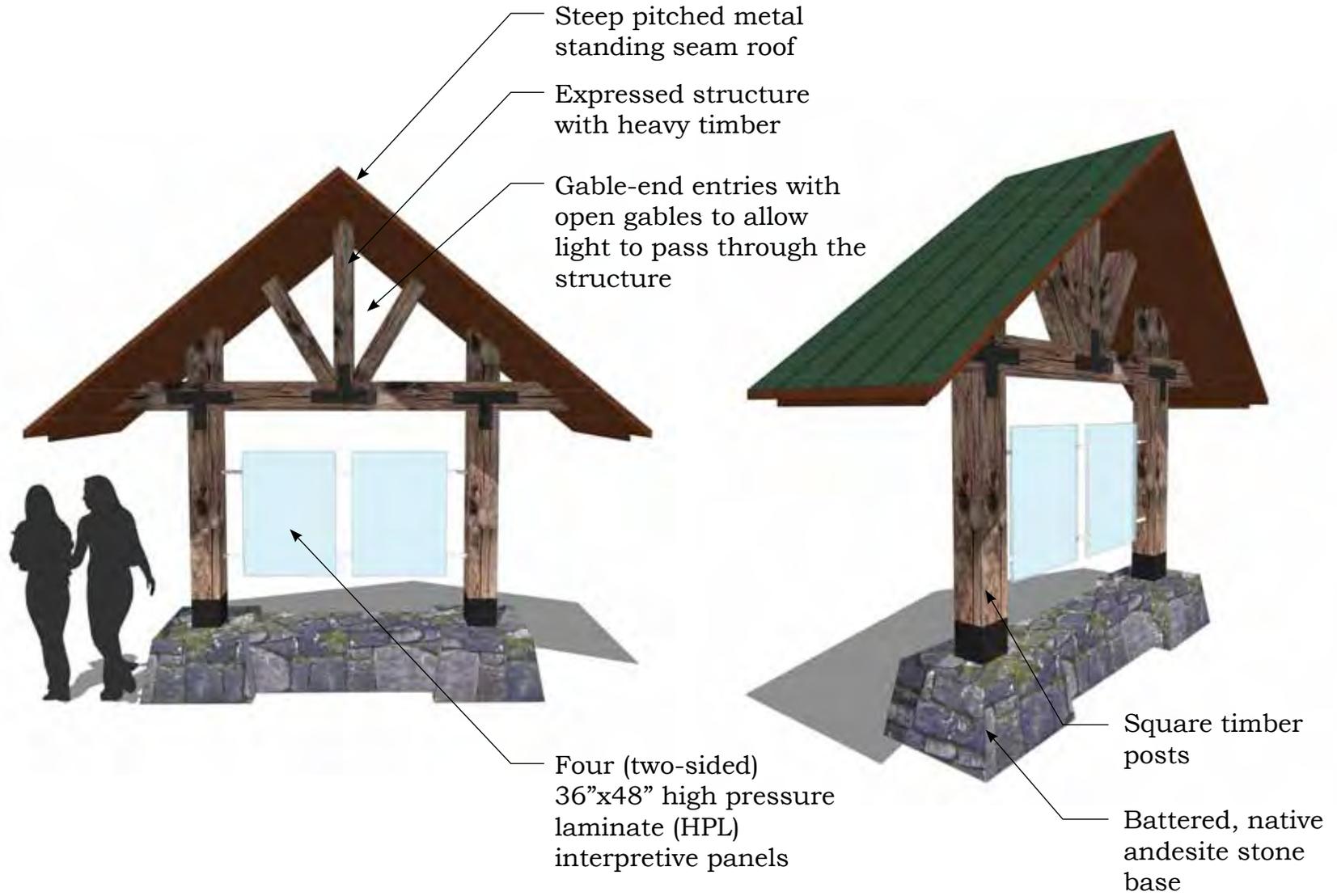
□ Bold roof line dominates composition

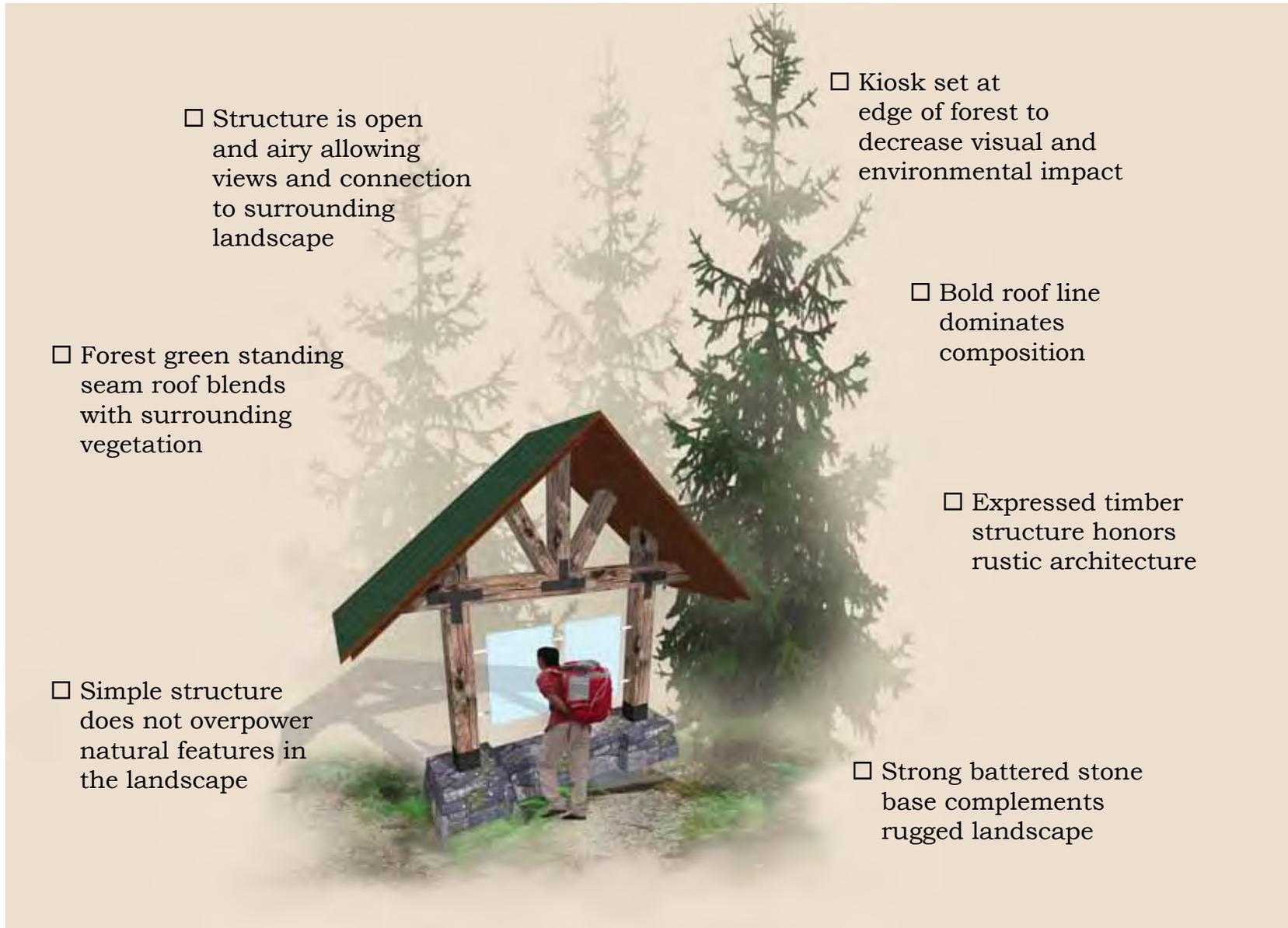
□ Exposed timber structure honors rustic architecture

□ Massing of timber posts blends with massive trees

□ Strong battered stone base complements rugged landscape

**Secondary Visitor Kiosk**





□ Structure is open and airy allowing views and connection to surrounding landscape

□ Forest green standing seam roof blends with surrounding vegetation

□ Simple structure does not overpower natural features in the landscape

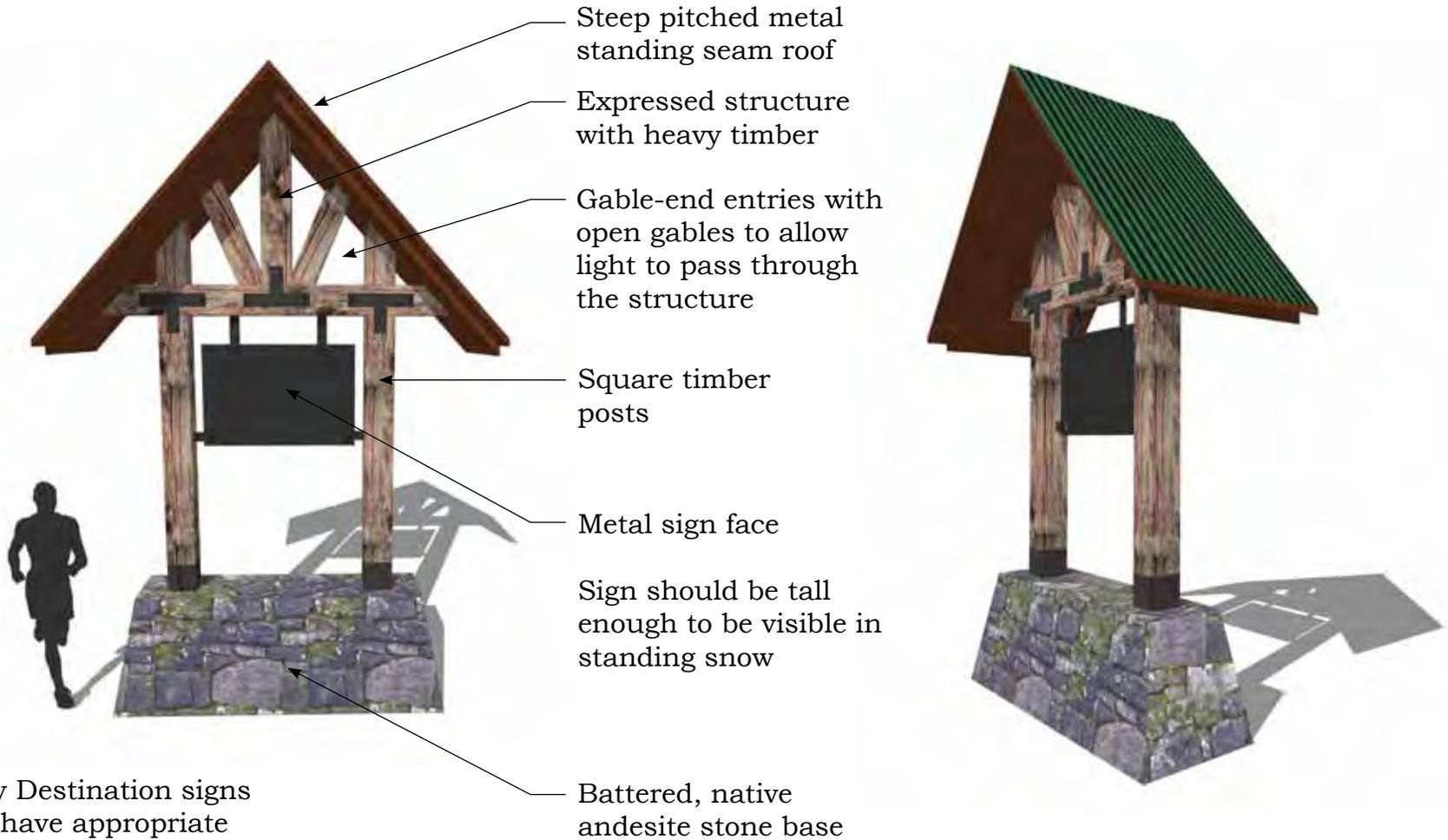
□ Kiosk set at edge of forest to decrease visual and environmental impact

□ Bold roof line dominates composition

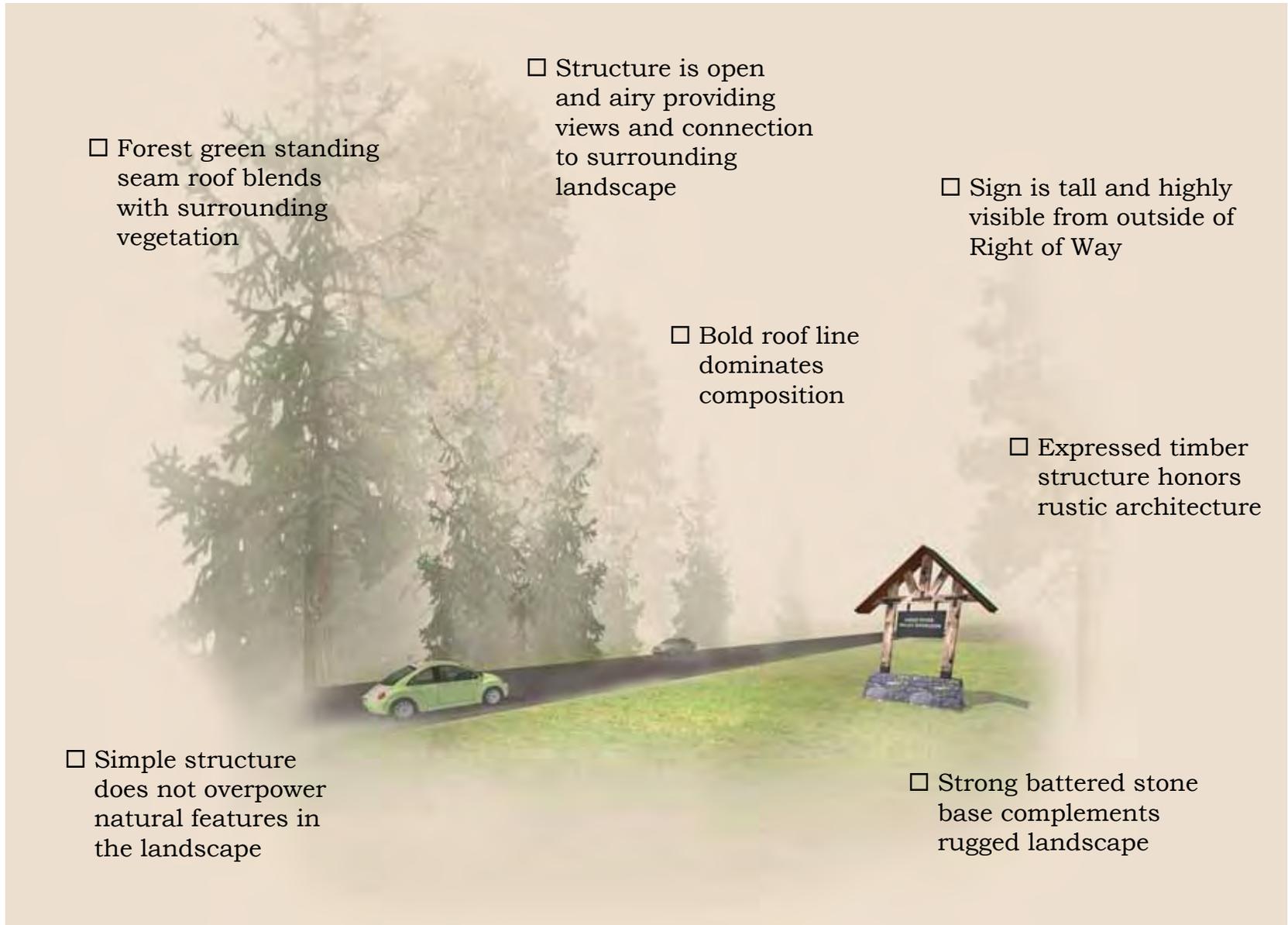
□ Exposed timber structure honors rustic architecture

□ Strong battered stone base complements rugged landscape

**Primary Destination Sign**



\*Primary Destination signs should have appropriate setback from road or appropriate vehicle safety barriers according to ODOT standards.



□ Forest green standing seam roof blends with surrounding vegetation

□ Structure is open and airy providing views and connection to surrounding landscape

□ Sign is tall and highly visible from outside of Right of Way

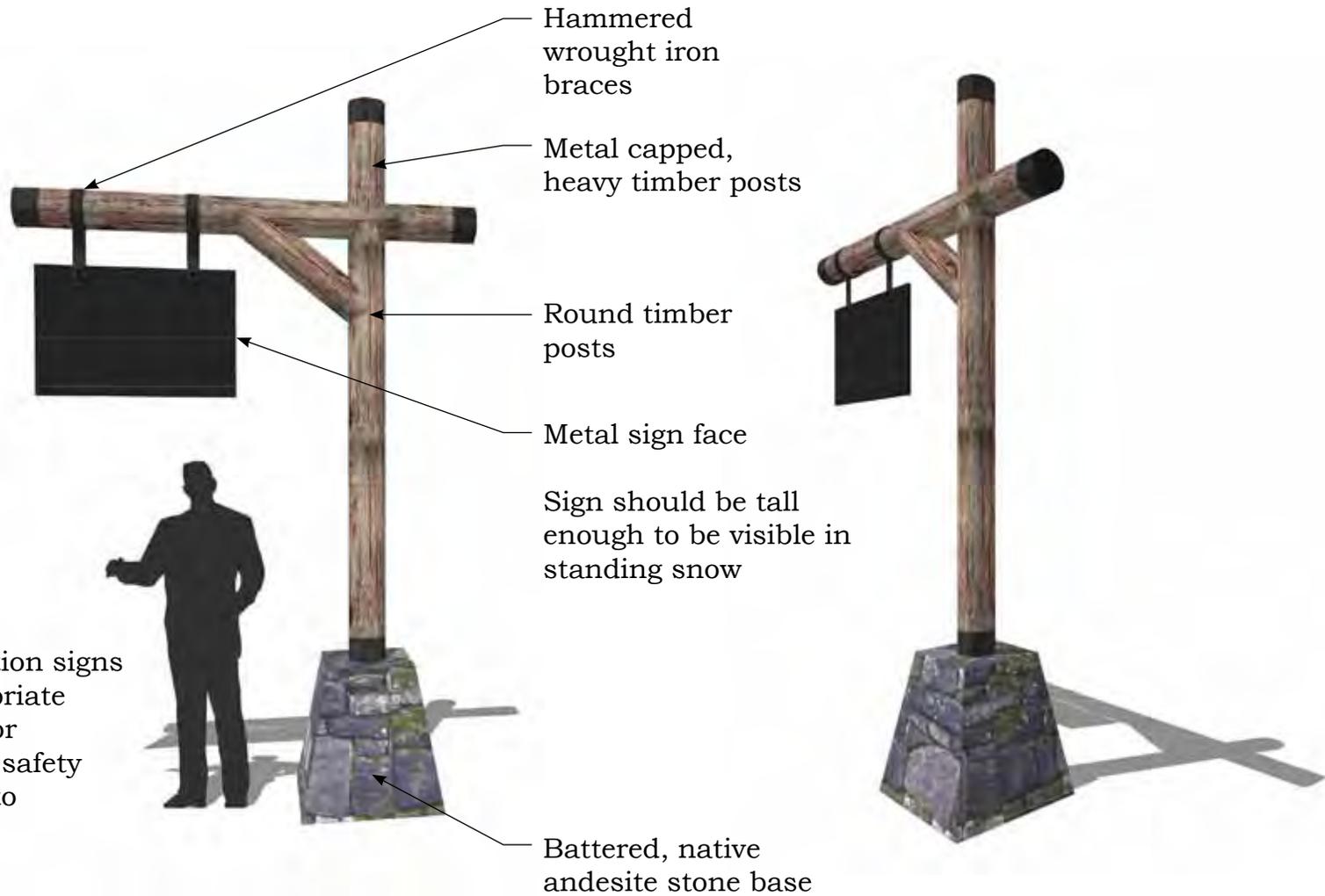
□ Bold roof line dominates composition

□ Exposed timber structure honors rustic architecture

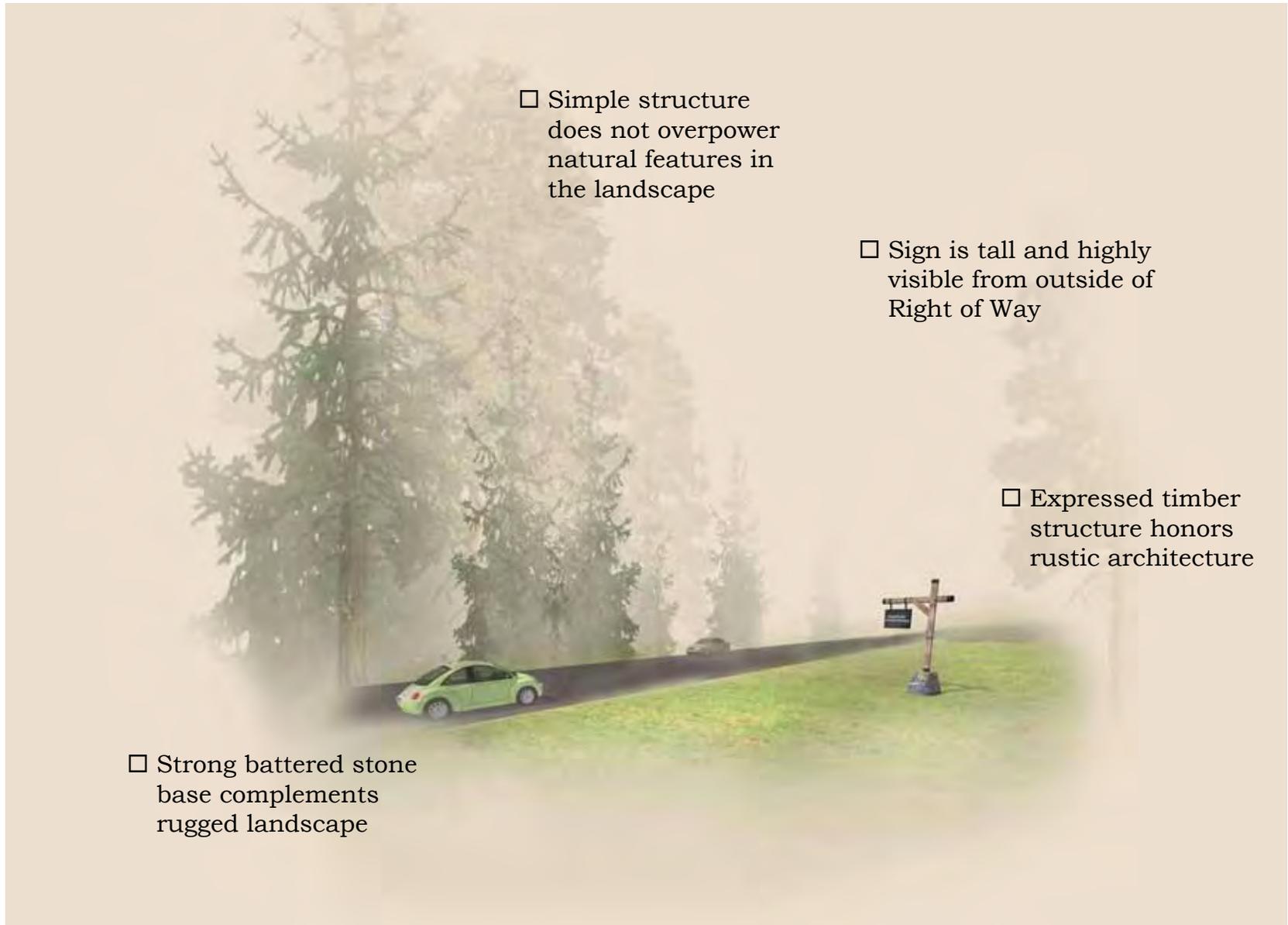
□ Simple structure does not overpower natural features in the landscape

□ Strong battered stone base complements rugged landscape

**Secondary Destination Sign**



\*Secondary Destination signs should have appropriate setback from road or appropriate vehicle safety barriers according to ODOT standards.



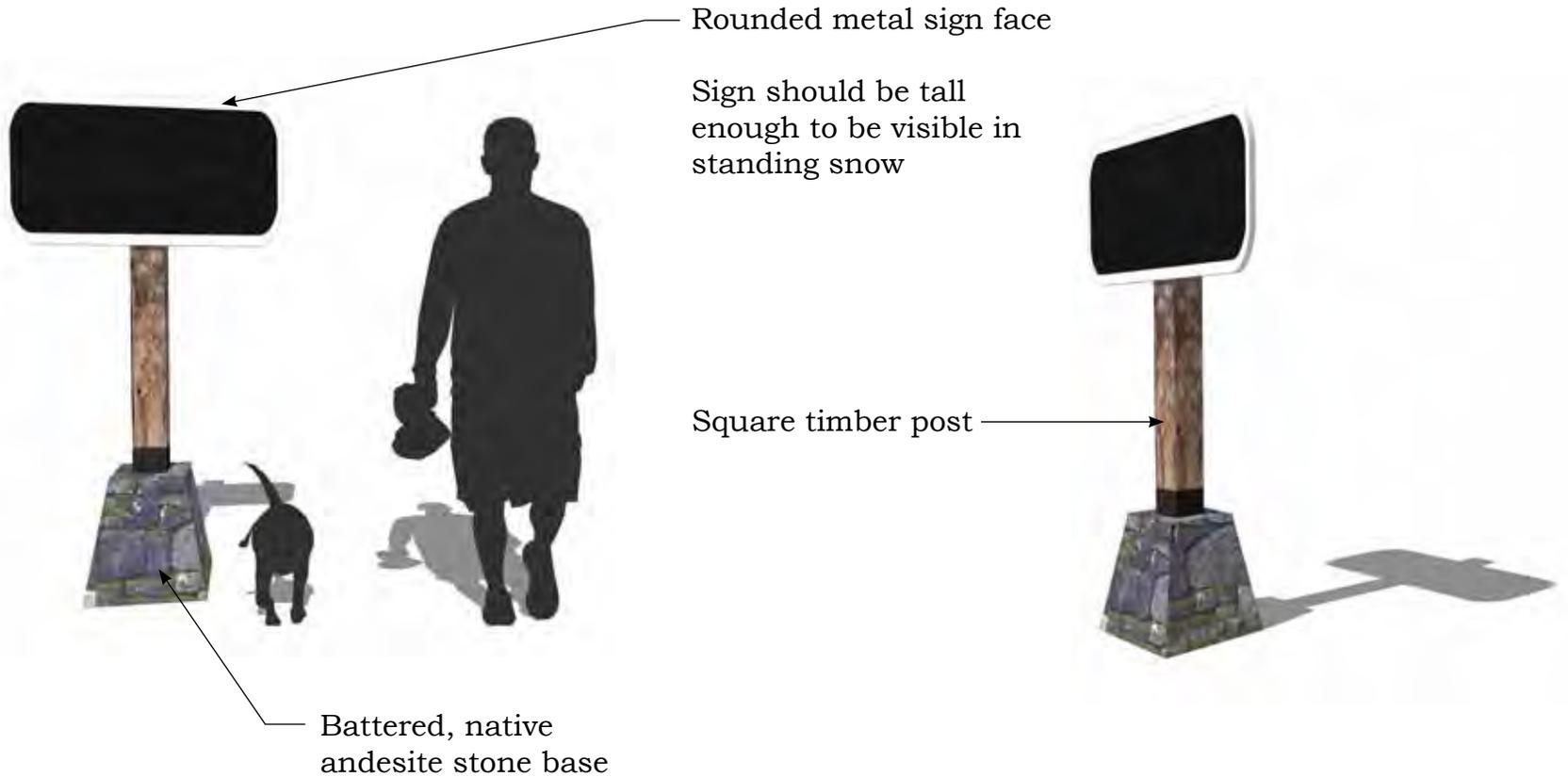
□ Simple structure does not overpower natural features in the landscape

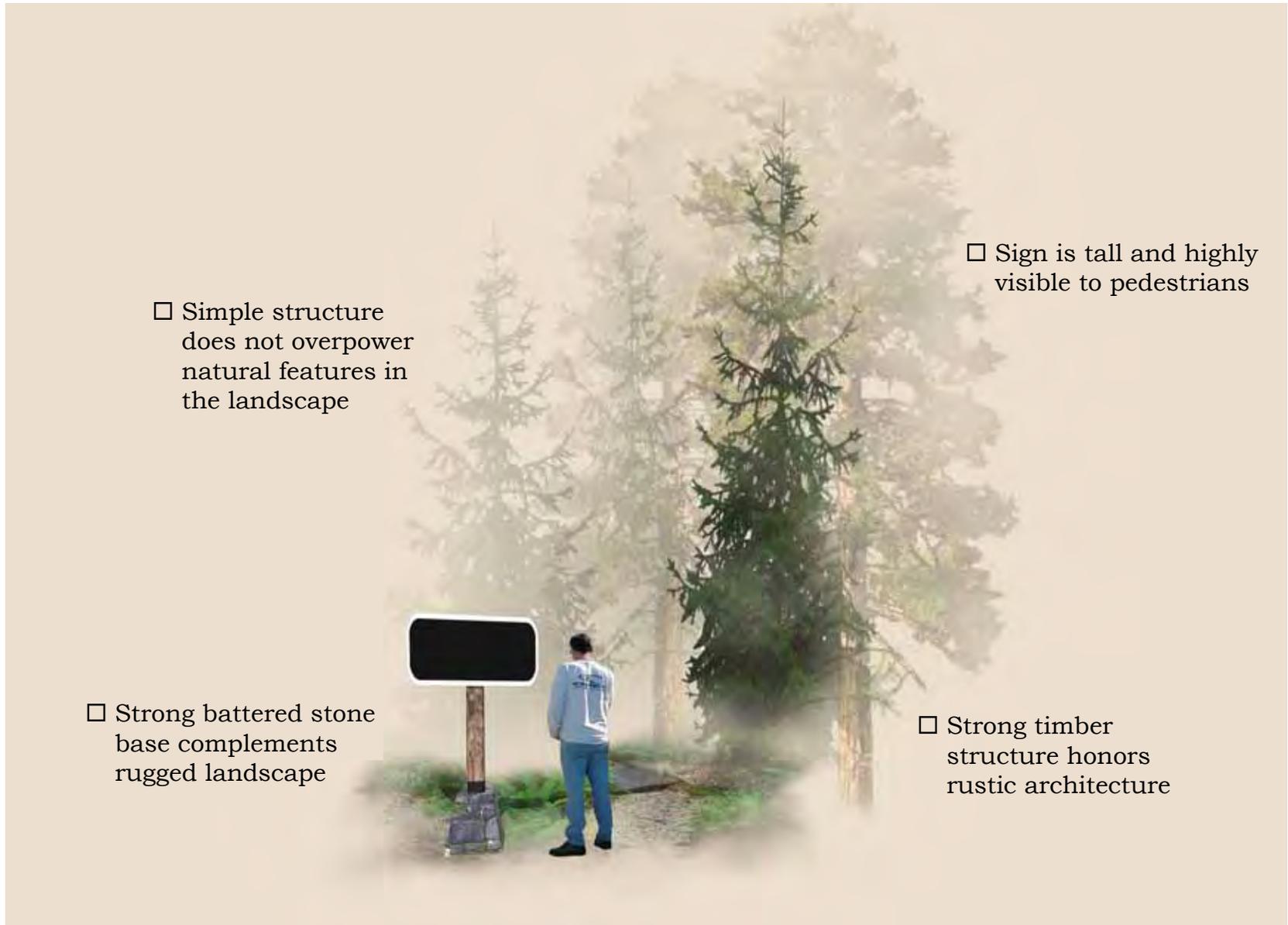
□ Sign is tall and highly visible from outside of Right of Way

□ Exposed timber structure honors rustic architecture

□ Strong battered stone base complements rugged landscape

*Trail/Informational Marker*





Simple structure does not overpower natural features in the landscape

Sign is tall and highly visible to pedestrians

Strong battered stone base complements rugged landscape

Strong timber structure honors rustic architecture

***Low Profile Exhibit***

- Strong battered stone base added to standard Forest Service Low Profile Exhibit stanchion to complement rugged landscape



***Forest Service Sign***



- Strong battered stone base added to standard Forest Service sign to complement rugged landscape

## NOTES



APPENDIX 71

## NOTES

## APPENDIX A: MT. HOOD SCENIC BYWAY CMP - DESCRIPTION OF INTRINSIC QUALITIES

The Mt. Hood Scenic Byway route encompasses a variety of intrinsic qualities making it interesting and unique to the traveler. These qualities include: Natural, Historic, Scenic, Agricultural and Recreational. The most unique qualities are those in the natural and historic categories. The natural wonders of the Columbia River Gorge, created by the Bretz/Missoula or Ice Age Floods, and Mt. Hood, an “at rest” volcano and highest peak in Oregon, have long been considered sacred by the Native Americans who first inhabited and explored these lands. The documented history of this part of the United States goes back to before the first humans of about 10,000 years ago to the activity of the last ice age floods that created this landscape. Much of our heritage is found in the many people who have called this area home. From the proud Native American people who fished, hunted, farmed and lived these lands to the explorers from many European countries. Their determination and bravery led to President Thomas Jefferson sending the Corps of Discovery, and

soon wagon after wagon of pioneers were making their way to the “promised land” of the Oregon Territory and passing right through the Mt. Hood Scenic Byway.

### Natural Qualities

The Mt. Hood Scenic Byway features numerous natural qualities, most notably Mt. Hood, Oregon’s highest peak, towering at 11,245 feet. A large portion of the byway is located in the Mt. Hood National Forest, encompassing more than one million acres. There are 189,200 acres of designated wilderness in the Forest. The largest is the Mt. Hood Wilderness, which includes the mountain's peak and upper slopes. Others are Badger Creek, Salmon-Huckleberry, Hatfield, and Bull-of-the-Woods wildernesses. The National Forest includes three designated National Wild and Scenic Rivers, the upper Sandy River, Salmon River and the White River. The Forest contains old-growth stands of fir, managed re-forestation, and native plants and foliage. The mountain area is home to alpine meadows, lake basins (natural and man-made), towering river canyons, and waterfalls.

Peaks, cliffs, and slopes give the route many of its most impressive and appealing features. Glaciers have eroded much of the mountain landscape and helped to create awesome natural vistas and formations. Throughout much of the area there is visual evidence of the mountain's volcanic origin. Mt. Hood has erupted repeatedly for over 500,000 years. For the past 30,000 years, lava-dome growth and collapse have sent fast-moving pyroclastic flows down the volcano's steep flanks, melting snow and ice. An eruption at Mt. Hood in the late 1700s choked the Sandy River with debris. In 1805, Lewis and Clark named this sand-choked river "Quicksand River." Since 1805, the river has flushed the excess sediment out. Over the years, floods and lahars have carved out the White River Canyon, with the most recent outburst flood occurring in 1998. Mt. Hood shows no sign of imminent volcanic activity today, but hot gases rising from fumaroles near Crater Rock and yearly swarms of small earthquakes indicate that it will not be quiet forever.

Cascade Streamwatch at the Wildwood Recreation Site in Welches, managed by the Bureau of Land Management (BLM), is a unique 580 acre day-use site with

more than five miles of forest hiking/walking paths along the Salmon River. Cascade Streamwatch is both a unique place and an innovative program that is a collaborative effort by BLM, USFS, and the non-profit educational organization Wolfree, Inc., that created one of the premier environmental education sites and programs in the Pacific Northwest. Accessible facilities include outdoor study areas, barrier-free interpretive trails, shelters, a wetland boardwalk trail, and an underwater stream profile chamber. The highlight of the Cascade Streamwatch trail is a stream profile viewing chamber where visitors can see an underwater "fish eye" view of a stream and anadromous fish habitat through a window built into an actual streambed. The focus of the self-guided interpretive trails and education programs offered at Cascade Streamwatch is river, stream, wetland ecological systems, and the effects of people on a natural forest setting.

The dense forests of the Mt. Hood National Forest and pristine waters of the Sandy, Salmon, Zig Zag, White and Hood Rivers and numerous mountain streams provide homes for many species of fish, wildlife and plants. Animal species that call

this area home include deer, elk, black bear, coyotes, beaver, reptiles, and over 150 species of birds. Highway 26 and 35 are part of the newly-designated Oregon Cascades Birding Trail, a self-guided auto tour that highlights selected sites for observing birds. The Oregon Cascades Birding Trail links a network of observation sites and birding experiences by using existing roads, pull-offs, parking areas, and walking trails to showcase birds in their natural habitats, specifically the Golden-crowned Kinglet, Swainson's Thrush, Winter Wren, MacGillivray's Warbler, the Mountain Chickadee, the Great-blue Heron, the Northern Goshawk, and hundreds of other distinct species of migratory and resident birds. These places also provide wonderful opportunities to see other wildlife such as deer, fish, and butterflies living amidst the beauty and wonder of Oregon's natural landscapes.

Vegetation along the route varies along the suburban valleys all the way up the western slopes, around the south side to the summit of Mt. Hood and back down the eastern slopes to the fertile Hood River Valley and the Columbia River Gorge. Pastures and fields dominate the western valley but are replaced with dense forests

and native alpine flora and fauna as the traveler ascends the mountain. Forests are composed of towering Douglas fir, noble fir, western cedar, hemlock, spruce, maple, alder, white oak, ponderosa pine, and tamarack. Vine maple, moss, ferns, rhododendrons, wildflowers and Oregon Grape (the state flower) carpet the forest floor with a rainbow of changing colors as the seasons transpire on the mountain.

As one of Oregon's smallest counties, Hood River County offers an enormous range of geological and climatological features. The City of Hood River, at the northern border of the county, sits at just 100' above sea level, while the county's southern border soars to the 11,245' summit of Mt. Hood. This drastic elevation change makes for an exciting drive and an extremely compact diversity of terrain.

Geologically speaking, Hood River is located within an area of the most important rock formation of the Northwest, known as the Columbia Lava. This formation is a vast sheet of basalt covering nearly 250,000 square miles and varying in thickness from 300 to 4,000 feet. This mass was not formed in one single volcanic eruption, but rather by a

series of several eruptions from Mt. Hood and Washington's Mt. Adams. Since its formation, the Hood River Valley has been modified in detail by erosion, both by running water and glaciers. The columnar cliffs of the Columbia River Gorge, visible along the Hood River often reach 500 feet or more in height. Other interesting rock formations of the area include more recent lava flows located west of the town of Parkdale. Thunder egg formations may be found in some areas of the East Fork of the Hood River.

### **Historic Qualities**

The Mt. Hood Scenic Byway is a historic route that ties geological, Native American, Lewis and Clark, and Oregon Trail history together. Mt. Hood was first seen by early explorers of European descent on October 29, 1792, by WR Broughton and his men of the Vancouver Expedition. Broughton named it Mt. Hood in honor of Lord Hood of British Admiralty. It wasn't until 1805 that the Columbia River Valley was explored as the Lewis and Clark Expedition paved the way, followed by trappers, fur traders, and pioneer settlers.

The Byway begins in Troutdale at the west end of the Historic Columbia River Highway along the river Lewis and Clark named the Quicksand River, now known as the Sandy River. Troutdale Historical Society owns and operates the Harlow House Museum, constructed in 1900 by Fred E. Harlow, son of Capt. John Harlow, Troutdale's founder. The Society also operates the Rail Depot Museum, a 1907 former Union Pacific Railroad depot.

Ascending the hill from the Sandy River bottom to the city of Sandy, the traveler will gain an appreciation for the experience the pioneers faced as they made the arduous trek along the Barlow Road, the last leg of the National Historic Oregon Trail. The bluff at Jonsrud Viewpoint in Sandy provides a spectacular vantage point for viewing the portion of the Sandy River the pioneers crossed on their route over the mountain, the same route the traveler is exploring. The Sandy Pioneer Museum (currently under construction) is dedicated to the preservation of the pioneer experience as well as the story of the timber industry, once the economic engine of this community. A brief side trip of 6 miles to the Philip Foster Farm National Historic Site in Eagle Creek is a

worthwhile trip to experience the first farm pioneers encountered as they neared the end of the Barlow Road. The Foster Farm hosted an estimated 10,000 exhausted pioneers on their trek west. Travelers can explore the house, barn, pioneer store, blacksmith shop and take part in the daily chores and activities that were the early pioneer way of life.

As the traveler continues east on Highway 26, the highway crosses and sometimes follows the route of the historic Barlow Road, a toll road that gave the pioneer wagon trains the option to finish the journey to Oregon City (end of the Oregon Trail) on land instead of floating the perilous Columbia River. The original Barlow Road cut right through the heart of the area. Travelers can see a replica of the original toll gate standing between two maple trees planted by Daniel Parker, the last gatekeeper from 1883-1902. West Barlow Tollgate was the last tollgate operated on the Barlow Road.

Pioneers used ropes and sheer strength to lower their wagons down the near-vertical slopes on their way to make land claims in Oregon City. Just a few miles east along Highway 26, travelers can walk a short

trail to view rope burns on the boulders and the drop pioneers faced at Laurel Hill Chute, the worst part of the Oregon Trail according to the diaries of the pioneers.

Government Camp's name comes from its history when an incident in the winter of 1849 forced a small command of the U.S. Mounted Rifles to abandon their supplies, wagons and livestock, and barely escape with their lives after an early winter storm caught them off guard. In Government Camp, the Mt. Hood Cultural Center and Museum is the centerpiece of both culture and history for the community at the base of Mt. Hood.

Timberline Lodge, built in the 1930s through the Works Progress Administration (WPA) and the Civilian Conservation Corps (CCC), is now a National Historic Landmark visited by over one million people a year. The lodge was built entirely by hand from materials hewn from the mountain, by laborers turned artisans. Art is present in every detail of the lodge, from the mosaic floor in the foyer, the hand-woven spreads and curtains, to subtle carvings in the beams and hand-made wrought iron door handles. The Rachael Griffin Historic

Exhibition Center features an authentic re-creation of a guest room as it would have appeared at the 1937 dedication.

The wagon ruts, the little grave yard at Summit Meadow near Trillium Lake, and the Pioneer Woman's Grave (access may be limited due to snow pack) all whisper to the visitor of stories and tales from long ago. Logs placed side-by-side formed a "corduroy" road across the swampy ground of the Meadow. From Summit Meadow, the highway again begins its climb to the summit of Barlow Pass, and travelers will marvel at how the pioneers with their wagons and oxen made such a trip. Near the interpretive sign, the Barlow hiking trail continues along the emigrant route, descending the hill about a mile to the Pioneer Woman's Grave. In 1924 survey crews building the Mt. Hood Loop Highway discovered the grave of a pioneer woman buried in a wooden box. The simple grave is now marked by rocks piled high by visitors as a memorial. Along the trail, wagon swales and rut traces can be seen. Even walking a few paces down the trail into the forest, the traveler will feel as though they've entered another time.

As travelers descend into the Hood River Valley and enter the Columbia River Gorge National Scenic Area, they can share in the appreciation of the extremes and beauty of the region expressed by Meriwether Lewis who wrote this journal entry on April 14, 1806:

The wind arose and continued hard all day but not so violent as to prevent our proceeding... the mountains through which the rivers passes nearly to the sepulchre rock, are high broken, rocky, partially covered with fir white cedar, and in many places exhibit very romantic scenes. Some handsome cascades are seen on either hand tumbling from the stupendous rocks of the mountains into the river.

Settlement of the Hood River Valley began in 1854, bringing the first plantings of fruit trees. The Oregon Short Line Railroad was completed in 1884 and connected to the Union Pacific Railroad, bringing rail transportation to the valley. Fruit production then became the major agricultural enterprise, hand-in-hand with timber production. During the years of 1890 to 1920, Hood River became world famous for its apples. In 1919 a

disastrous freeze killed many apple trees and most farmers chose to plant pears as they replaced their orchards. Hood River County is now the leading producer of Anjou pears in the world.

In Parkdale, the Hutson Museum offers exhibits on Native American art and basketry from the area, as well as great interpretive exhibits of the area's unique geological features. In the community of Mt. Hood, the historic Mt. Hood Towne Hall and the Parkdale Ranger District each offer visitor information services. In Hood River, the Hood River Hotel, the Columbia Gorge Hotel, and the National Historic District of the Mount Hood Railroad are all listed in the National Register of Historic Places and are all worthy of exploring. The Hood River County Historical Museum offers fascinating exhibits of the area's agricultural heritage, as well as modern exhibits on the evolution of the sport of windsurfing in the Gorge.

The Mt. Hood Scenic Byway ends at the Historic Columbia River Highway National Historic Landmark. Travelers can follow this historic route either west, into downtown Hood River, or east, to the Senator Mark O. Hatfield West Trailhead.

Beyond the Trailhead, the historic highway is an accessible, multi-use trail providing access to the recently re-opened Mosier Twin Tunnels.

For a list of the many sites and structures designated as National Historic Places and Landmarks see Appendix G1.

### **Scenic Qualities**

While on the Mt. Hood Scenic Byway, travelers leave the suburban atmosphere of East Multnomah County and enter into a more quiet and tranquil way of life. Dotted the Byway are small family farms, and nurseries where trees, shrubs and flowers of every kind are grown for shipment all over the world. The traveler can catch glimpses of Mt. St. Helens, Mt. Adams and the Washington Cascades to the north while Mt. Hood beckons from a distance. From this pastoral way of life, the road suddenly descends into a primeval forest, complete with canopies of huge old growth fir and deciduous trees, small waterfalls springing from what seems like nowhere, and moss hanging from every branch.

The route then comes to Dodge Park featuring huge viaducts from the Bull Run Reservoir. The viaducts supply metropolitan Portland with its drinking water. The traveler again crosses the Sandy River as it races toward its final destination of the Columbia River and finally the Pacific Ocean. Crossing the Dodge Park Bridge, many find this a very popular fishing spot. From this point, the route again changes back to the small farms and passes Roslyn Lake. Not to be missed is Oral Hull Park for the Blind, a historic 23-acre park and camp designed especially for the visually impaired, emphasizing use of all five senses.

The route now takes on a more aggressive nature, as the traveler crosses the Revenue Bridge and starts the climb from the river bottom to the top of the valley and into the city of Sandy. This is the gateway to the Mt. Hood National Forest and a stop at Jonsrud Viewpoint will give the traveler a visual understanding of what the emigrants on the wagon trains encountered as they traveled the Barlow Road. Clear sightlines to Mt. Hood and surrounding areas from the Viewpoint are an added scenic bonus.

As the route continues east on Highway 26 toward the gateway to the Villages of Mt. Hood, (Brightwood, Welches, Wemme, Zig Zag, Rhododendron and Government Camp,) the highway crosses and sometimes follows the historic Barlow Road. Here the route becomes a haven for the person seeking outdoor activities. Located right in the heart of the Mt. Hood National Forest, there are the year-round recreational opportunities and the absolutely breath taking, ever-changing, dramatic and inspiring view of the mountain itself.

A short side excursion not to be missed is Timberline Road and the awesome ascent to Timberline Lodge National Historic Landmark, sitting at the 6,000-foot level of Mt. Hood. Built in the 1930s through the efforts of the WPA and CCC, Timberline Lodge proudly stands as the crown jewel of Mt. Hood, showcasing the Cascadian mountain architecture so proudly created through the handiwork and artisan craftsmanship of many during the depression era.

Connecting to Highway 35 and heading north towards Hood River, the route again begins its final ascent to the summit

of Barlow Pass. On the descent, White River Canyon comes into view with yet another jaw-dropping view of Oregon's highest peak. Just north of the Mt. Hood Meadows access road, travelers can access Sahallie Falls for a brief respite and photo opportunity before continuing on their journey. Surrounded by the majestic mountain valleys and ridges, tree-covered with creeks, springs, and rushing white water, the route drops from the summit to the meadows of the north slope of the mountain.

From the canyon with its views of both forks of the Hood River, Polallie Creek, rock walls, towering trees, and teeming wildlife, the road suddenly opens up to the Hood River Valley vistas. As travelers enter this bustling agricultural district, with its quiet, pastoral charm, they will see why the Hood River Valley is such a popular destination for city dwellers seeking a taste of life in the country.

The communities of Mt. Hood, Parkdale, Dee, Odell and Pine Grove welcome travelers to the vistas of orchards, farms and vineyards all waiting to be explored and indulge the senses. Travelers delight at the opportunities to view Washington's

majestic Mt. Adams, while Mt. Hood is still close enough to appear in the rear view mirror. A quick side trip up to Panorama Point offers the best views of the Valley, with the Gorge and Mt. Hood both framing the vista.

Arriving at the shores of the mighty Columbia River, with the historic and trendy shopping district of downtown Hood River immediately to the west, the Mt. Hood Scenic Byway draws to an end at the intersection of Highway 35 and the Historic Columbia River Highway. Travelers will now find themselves at the panoramic crossroads of the Columbia River Gorge and the Cascade Mountain Range – where a world of outdoor recreation, scenic pleasures, historic landmarks and friendly people welcome visitors to explore in the footsteps of Lewis and Clark.

From here, travelers can continue their Oregon Scenic Byway experience by connecting directly to the Historic Columbia River Highway Scenic Byway, or the Journey through Time Scenic Byway, less than an hour away to the east.

### **Agricultural Qualities**

This Scenic Byway beckons travelers to enjoy the bounty of Oregon. This is after all part of the highly publicized “Garden of Eden” that beckoned so many to make the great Westward Migration to The Oregon Territory.

The Byway is dotted with the local farms and nurseries that proudly grow and craft our agricultural treasures including Scenic Fruit Co cannery and Oregon Bulb Farm. As the traveler leaves the suburban atmosphere they are greeted with a landscape of a country patchwork of small family farms, ranches, fields for livestock, and nurseries where trees, shrubs and flowers of every kind are grown for shipment all over the world. Clackamas County is known as the largest producer of nursery stock and Christmas trees in the U.S.

There are many wineries along the route, beginning with McMenamins Edgefield as you leave Troutdale. Sandy is home to Wasson Brothers Winery, the oldest winery in the Mt. Hood Territory and offers unique handcrafted berry and fruit wines, in addition to the more traditional

grape varieties. As the traveler descends Mt. Hood to the Hood River Valley, more wineries dot the landscape, specifically Cathedral Ridge Winery, Pheasant Valley Vineyard and Winery and Hood River Vineyards.

Culinary delights are found at dining establishments throughout the area. Many of the restaurateurs in the Villages of Mt. Hood use the term “Peak Cuisine” to refer to their special offerings and Cooking in the Country demonstrations with selected Chefs are very popular. The many fine restaurants of Hood River take pride in using locally grown and foraged produce in their menus. Travelers should not miss sampling a pear dumpling during harvest season – many restaurants feature this local favorite that is sure to bring them back for more.

The Hood River Valley can in many ways be considered Oregon’s fruit basket. Travelers will enjoy lavish vistas of orchards full of pears, apples, peaches, apricots, as well as vineyards and farms raising vegetables, flowers, alpacas, llamas and more. Nearly 50% of the nation’s winter pear crop is grown in this fertile and scenic valley, currently comprised

of about 380 commercial farms, sharing more than 14,000 acres in orchard land and 20,000 in farm land. Many orchards are open and welcome visitors during the spring, summer, and fall harvest season. Every April, during the Hood River Blossom Festival, the trees of the valley burst into bloom, carpeting the valley in dainty pink and white blossoms. In October, visitors are encouraged to join the locals in celebrating the bounty of the harvest season, as trucks deliver the fruits of the year's labor to distant lands and local dinner tables. The annual Hood River Valley Harvest Festival is the largest annual event in the Gorge, with art, wine, and the freshly harvested produce attracting visitors from throughout the Pacific Northwest. The Mt. Hood Railroad offers train excursions into the valley to enjoy the scenic views of the orchards during most of the year.

### **Recreational Qualities**

The Mt. Hood Scenic Byway provides access to the most popular recreation destination in Oregon, offering a wide range of outdoor settings for year-round recreational activities. From year-round

skiing on Mt. Hood at Timberline, to world-class wind surfing in Hood River, the route is known around the world for top-notch recreational offerings. The Pacific Crest Trail along the Cascades is accessible along the Byway at Barlow Pass on Highway 35 and Timberline Lodge, and many viewing areas of the Oregon Cascade Birding Trail's – Mount Hood Loop are also along the route.

Travelers can enjoy fishing, camping, picnicking, rafting, kayaking, boating, swimming, biking, climbing and hiking in the summer; hunting in the fall; and skiing, snowboarding, snowmobiling, snowshoeing, sledding, dog sledding (tours available), and sleigh rides in the winter. Trails for hiking, mountain biking, horseback riding, Nordic skiing and snowshoeing can be found throughout the area. Berry-picking and mushroom collecting are popular activities, and for many families a trip in December to cut down the family's Christmas tree is a long-standing tradition. Beginning with the most basic of hiking trails and running the gambit to trails for experienced hikers and climbers and to extreme sports, the opportunities for all ages and experience levels await the traveler along the route.

The ski resorts along the Byway include: Mt. Hood Skibowl with the largest lighted night ski area in U.S. and 4 chair lifts and 4 tows; Summit Ski Area with areas for tubing and beginners on the slope and located in the heart of Government Camp; Timberline Ski Area with summer runs on Palmer Glacier as well as winter runs from 6 lifts; Mt. Hood Meadows Ski Resort the largest alpine recreation area with 2,150 acres of terrain and 11 lifts; and Cooper Spur Mountain Resort with a more rustic traditional winter recreation experience and Tubing Center.

White River Canyon and nearby Bennett Pass have also become a mecca for snow sports enthusiasts of every kind, with cross-country skiers, snowmobilers, snowshoers, and sledders all enjoying the easy access and expansive terrain. Snow-park permits available at retailers and information centers throughout the area are required for parking as designated in Mt. Hood National Forest. Snow-park areas for snow play include Summit, Snow Bunny, White River Canyon, Tea Cup Lake, Clark Creek, Glacier View, Pocket Creek, and Little John.

The region's numerous lakes, rivers and streams provide outstanding fishing, boating and rafting opportunities. Trout, Chinook salmon, and steelhead are actively pursued throughout the fishing seasons. Travelers can try their hand at fly fishing on the pristine mountain streams. Travelers ready to venture into the area can contact one of the area's many professional guide services and outfitters for assistance. White water rafting and kayaking have become popular sports in the Mt. Hood National Forest, as well as along sections of the Hood River.

With 10,000 summit attempts per year, Mt. Hood is the second most-climbed mountain in the world, surpassed only by Japan's Mt. Fuji. Climbing season is mid-April to mid-June, and wilderness and climbing permits are required. Climbers are requested to carry a Mountain Locator Unit, register at Timberline Lodge before their climb and sign back in upon return to assist searchers if a search and rescue is necessary.

Hood River is known as the windsurfing capital of the world. New to the scene is kiteboarding. It is not uncommon to see vehicles with snow skis and a windsurfing

board strapped on top. There is no other place where visitors can ski in the morning on the slopes of Mt. Hood and then windsurf in the afternoon on the Columbia River. Hosting of the annual Gorge Games has played a significant role in raising the awareness of the Columbia River Gorge as premier destination for windsurfing and sailing.

The Mt. Hood Scenic Byway offers camping at numerous public campgrounds for tent camping and R/V parking as well as wilderness camping experiences. There are five campgrounds in the Mt. Hood National Forest that are visible along the Byway, with an additional six that are located in close proximity (within 5-6 miles) to the route. There are a total of eighty designated campgrounds in the National Forest. Camping is also available at city and county parks along the route, specifically Dodge Park, Routson Park, Tollbridge Park, Lost Lake, Kingsley Reservoir, Laurance Lake, and Tucker Park. Additional camping opportunities are easily accessible in the nearby Columbia River Gorge National Scenic Area.

For those travelers seeking recreational experience, off the mountain, there are many offerings. Golf is a popular recreational activity with public courses throughout the area. The Resort at The Mountain offers the Three Nines (27 hole course) on the edge of the Mt. Hood National Forest. At Indian Creek Golf Course in Hood River, views of snowcapped Mt. Hood appear at every tee box. If miniature golf is more appealing, there are two courses, Mountain Air Miniature Golf Course and Mt. Hood Skibowl Summer Adventure Park. The Adventure Park also has over 20 summer attractions including a 1/2 mile dual Alpine Slide, Indy karts, bungee jumping, 40+ miles of mountain bike trails, bike rentals and tours, automated batting cages, and Frisbee golf.

The Byway route provides access to numerous festivals and events hosted within the local communities throughout the year, providing a celebration of heritage, local art, cuisine, entertainment and fun for families and visitors alike.

## NOTES

## APPENDIX B: MT. HOOD SCENIC BYWAY CMP - MARKETING STRATEGY

### Marketing Organizations

The Marketing Strategy for the Mt. Hood Scenic Byway is included in marketing plans prepared and managed by several local destination marketing organizations as well as the Mt. Hood - Columbia River Gorge Region Tourism Alliance. Oregon's Mt. Hood Territory, Columbia River Gorge Visitors Association, Hood River County Chamber of Commerce and West Columbia Gorge Chamber of Commerce all have marketing strategies for the area, including the scenic Byway route, that include advertising and media campaigns, as well as visitor information brochures and magazines to be used as fulfillment. The strategies are designed to create a destination that a visitor will want to explore for more than one day, thus increasing the visit stays to several days in the Byway area.

### Target Markets

Our target market for visitors to the Byway includes local residents who have

friends and relatives visiting, visitors to the metropolitan Portland area including convention delegates to the Oregon Convention Center and visitors to Oregon from around the world. The Portland International Airport is located just minutes from the start of the Byway and is served by 15 airlines including Lufthansa with direct service to Germany and an extensive connecting system reaching much of Europe, Africa and Asia. Northwest Airlines has direct service to Japan and connecting service throughout Asia. Mexicana Airlines has direct service to Guadalajara and connections throughout Mexico. Another focus of our marketing is the packaged travel market and most sightseeing motorcoach tours to Oregon already include a trip on portions of this Byway.

We plan to target the spring and fall months as prime times to enjoy the many natural and historic qualities of the Byway. This will assist in spreading the visitor traffic over the entire year and support growth and sustain current businesses. We will also focus our efforts on mid-week business as another target for growth. A group of properties in the Government Camp area have successful

launched mid-week programs for winter sports. Many people look for the less crowded time to travel as well as have work schedules that give them time off during the traditional work week. We also know that the international visitor will be able to visit throughout the year. Currently the summer snow boarding camps are bringing both boarders and parents from all over the world to the area for several weeks in the summer.

### **Competitive Analysis**

The competition for visitors to the Mt. Hood Scenic Byway depends on the experience the potential visitor is seeking to enjoy. For those already here in the metropolitan Portland area it is sometimes a choice between the drive to the Oregon Coast and the area covered by this Byway. However, the sights, and scenic beauty of the two choices are different and the history is tied together by the early tribal people and the explorers, so many visitors make time to have both experiences. Nationally, the competition is the other beautiful and scenic National Parks and designated Scenic Byways/All-American Roads. However, the experiences along the Mt. Hood Scenic Byway are not duplicated

in these other areas and with the world-wide reputation of the Gorge and Mt. Hood this area is on most “want to visit” lists. International visitors often choose to come for a visit to Oregon because of our scenic beauty and natural wonders so again the area is high on the “to do” list for these visitors along with the Oregon Coast, Crater Lake and Mt. St. Helens.

### **Marketing Strategies**

Current strategies for promotion of the Byway include the Barlow Road Driving Tour developed by Oregon’s Mt. Hood Territory. This is a 73-minute narrated script on CD with historical accounts of tales and diary excerpts along with themed music that follows the historic Barlow Road as much as is possible on today’s roads. As explained in the route description, this route is similar to the Highway 26 portion of the Byway. The CD is available to borrow or buy at the Mt. Hood Visitor Information Center and many other establishments along the route.

Another example is the Gorge Guide. This is an annual free magazine produced for the Columbia River Gorge Visitors Association. In 2005, 80,000 copies will

be printed and widely distributed within the region, across the Pacific Northwest and internationally. The magazine includes feature articles, photography and route maps for driving the region. It is supported by paid advertisements and features many of the facilities and services available to visitors in the region.

A similar publication is the Travel Guide for Oregon's Mt. Hood Territory. It is published annually by the Clackamas County Tourism Development Council with 50,000 copies available free to visitors to the region and additional copies used as fulfillment for all visitor inquiries resulting from national and international media advertising and publicity. This publication features stories about the Territory, photography and listings for all facilities, attractions and services in the area. There is no paid advertising in this publication.

Published by the Hood River County Chamber of Commerce, the "Discover Your Next Adventure" brochure features descriptive narrative, images, and listings for visitor service businesses. The narrative describes the outdoor adventure sports, agriculture tourism, and cultural/natural history characters of the

region. The map is supported with paid advertising and includes all of Hood River County and portions of Clackamas County roads and points of interest.

Travel Oregon's approved marketing plan for the regional group, Mt. Hood - Columbia River Gorge Tourism Alliance, combines the other scenic byways (Journey Through Time Scenic Byway and Historic Columbia Highway Scenic Byway/All-American Road) in the region to create an infinity (figure eight) loop tour. This loop tour has four lodging hubs in Troutdale, Hood River, The Dalles and the Villages of Mt. Hood. The attractions and services along the entire infinity tour route are being inventoried and will be featured in a printed brochure/guide and on a website. Funds are set aside for a fall 2005 campaign in the Seattle - Vancouver B.C. area to promote the tour and the brochure/guide will be used as a fulfillment piece.

## NOTES

**APPENDIX C: DELETED SITES**

(MAY BE CONSIDERED FOR FUTURE INCLUSION)

Site	Location	Subtheme(s) and Storyline(s)	Existing Media/Facilities	Recommendations and Notes
Sandy River Bridge East	Milepost 00.0	N/A	Columbia River Gorge National Scenic Area (CRGNSA)	
Glen Otto Community Park	Milepost 00.1	N/A	Columbia River Gorge National Scenic Area (CRGNSA)	
Sandy River Bridge West	Milepost 00.1	N/A	Columbia River Gorge National Scenic Area (CRGNSA)	
Harlow House Park and Barn Museum	Milepost 00.4	N/A	Columbia River Gorge National Scenic Area (CRGNSA)	
Troutdale Depot Rail Museum	Milepost 00.6	N/A	Columbia River Gorge National Scenic Area (CRGNSA)	
Troutdale Downtown City Park	Milepost 00.7	N/A	Columbia River Gorge National Scenic Area (CRGNSA)	
Roslyn Lake	Mileposts 17.9 & 18.0	N/A	None	Do not consider because the lake has been decommissioned. Move stories of river/fisheries restoration and inter-urban trolley line to Dodge Park.

Site	Location	Subtheme(s) and Storyline(s)	Existing Media/Facilities	Recommendations and Notes
Cliffside Cemetery	Milepost 19.2	N/A	None	Do not consider because of unknown significances. Unknown if Oregon Trail emigrants are buried here. In addition, there are better sites along the byway to tell this story
Mt Hood Visitor Information Center	Milepost 40.0	N/A		Do not consider- site closed
Ramona Falls Trailhead (Old Maid Flats)	Milepost 42.0	N/A	None	Do not consider due to its remoteness from the byway
Forest Road 39 (Twin Bridges Road or East Kiwanis Camp Road)	Milepost 48.6	1. B, C 2. A, F, G 3. C, D, E, H	Trailhead signage	Provide 3-panel kiosk w/ restroom- information will include: <ul style="list-style-type: none"> <li>• Orientation Map (including trailheads for major trails in area)</li> <li>• Know Before You Go- Safety and land use ethics</li> <li>• Trail information</li> <li>• Investigate need for left and right turn lanes, and for sight distance improvement.</li> </ul>
Dead Man's Curve	Milepost 49.7	N/A	None	Do not consider due to highway safety concerns

Site	Location	Subtheme(s) and Storyline(s)	Existing Media/Facilities	Recommendations and Notes
Glacier View Trailhead	Milepost 52.6	N/A	Trailhead, Trail signage, Portable restroom, picnic table	Do not consider - information regarding the trailhead is available at Government Camp
Skiway Trail Access	Milepost 53.7	N/A	None	Do not consider - information regarding the trailhead is available at Government Camp
Crosstown Trail	Milepost 54.1	N/A		
Snow Bunny Snow Park	Milepost 55.7	N/A	Parking and winter shelter	
Trillium lake Snow Park	Milepost 55.8	N/A	Parking	
Salmon Creek Ford	Milepost 57.5	N/A	None	Do not consider due to access issues
Switchback Falls	Milepost 63.3.	N/A	None	Do not consider due to safety considerations
Tea Cup Lake Snow-Park & Clark Creek Sno-Park	Milepost 65.3	N/A	Parking	
Pocket Creek Sno-Park	Milepost 67.1	N/A	Parking	
Gumjuwac Trailhead	Milepost 68.4	N/A	Trailhead parking	

Site	Location	Subtheme(s) and Storyline(s)	Existing Media/Facilities	Recommendations and Notes
Little John Sno-Park	Milepost 70.6	N/A	Parking, Restrooms, 2 picnic tables, fire pit, warming hut	
Road 44 Junction (Mt Hood Vista)	Milepost 70.8	N/A	Parking	
Sherwood Campground	Milepost 72.1	N/A	Developed campground with restrooms	
Poallie Trailhead (Cooper Spur)	Milepost 73.8	N/A	Parking	
Routson County Park	Milepost 76.2	N/A	Parking, picnic tables, restroom, water	Do not consider- nice site to stop along the byway but there is no significant interpretive opportunity
Baseline Drive Intersection	Milepost 80.1	N/A	Parking	
Parkdale – Bicentennial Memorial Gardens	Milepost 85.0	N/A	None	
Viewpoint at Milepost 82.9	Milepost 82.9	N/A	Parking	
Oak Ridge Trailhead	Milepost 83.7	N/A	Parking & trailhead signs	
Viewpoint at Milepost 92.5	Milepost 92.5	N/A	Parking	

Site	Location	Subtheme(s) and Storyline(s)	Existing Media/Facilities	Recommendations and Notes
Viewpoint at Milepost 100.5	Milepost 100.5	N/A	None	Do not consider - safety concerns regarding sight distance and high truck volume
Panorama Point	Milepost 101.6	N/A	Day Use site with restrooms and drinking fountain	Do not consider- for southbound travelers it would require a left hand turn into the facility. The facility is worn, dated and the parking area is in need of replacement
Button Junction Intersection, Oregon Hwy 35 & Hwy 30	Milepost 102.0	N/A	Columbia River Gorge National Scenic Area (CRGNSA)	Do not consider – site is too congested.
Tucker County Park	On Hwy 281 between Summit Dr & Wy'east Rd	N/A	None	